



# West Rosemary Development Guide



DRAFT March 17, 2017

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SARGENT

Noell Consulting  
Group

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# West Rosemary Development Guide

## Project Summary

Create a West Rosemary Street Development Guide (Guide) that sets forth a commercial and economic development vision for the West Rosemary Street corridor that is feasible from a market perspective and is consistent with the vision statements of the Northside Neighborhood Conservation District (NCD).

## Planning Question

Is it possible to develop a vibrant commercial and economic development corridor along West Rosemary Street that is consistent with the Vision Statements of the Northside Neighborhood Conservation District?

## Context

The Northside neighborhood is a historic African American neighborhood with a rich history tracing emancipation, reconstruction, Jim Crow, civil rights struggle, and desegregation. It is one of the most diverse neighborhoods in the region. While its proximity to the University and relatively affordable houses have made it an attractive location for off-campus student housing, neighborhood activists have been working to maintain the neighborhood's history and preserve the culture of a diverse, family oriented neighborhood.

## Area Description:

West Rosemary Street, which is the northern edge of the downtown Chapel Hill (Downtown) area, is located along the southern edge of the Northside Neighborhood Conservation District (see map on the following pages). West Rosemary Street runs parallel to Franklin Street, bordered on the western end by South Merritt Mill Road and on the eastern end by South Columbia Street and is located in the Downtown Service District. Downtown is the heart of Chapel Hill with a core of mostly 2-3 story buildings, many of which are historic. Over the past few years, Downtown has experienced change and new development which differs from the

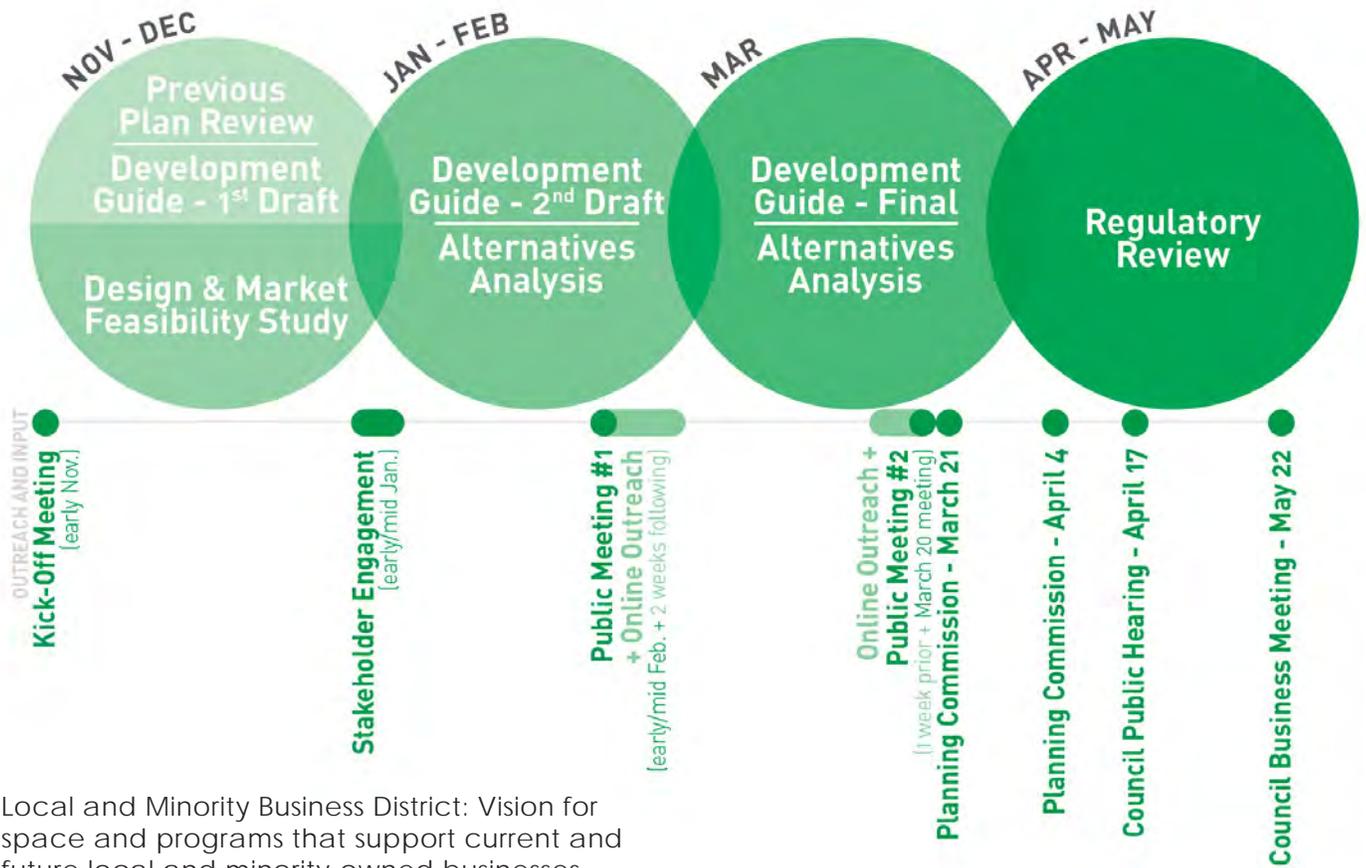
historic structures; for example, recent additions include 140 West and Greenbridge—both about 8-10 stories in height. Carolina Square, with a projected opening of October, 2017, will have one 11-story building and two 5-story buildings.

West Rosemary Street lies within the boundary of the Northside Neighborhood Conservation District (NCD) and the Downtown Service District. The goals of these districts have set up a tension between preservation and economic development goals. Previous planning initiatives that tried to create a boundary between the two with buffers and zones seemed to exacerbate this tension because there was a perception of winning and losing. Therefore, as an alternative approach, the Town is interested in achieving more community consensus by removing the idea of a "boundary" and trying to apply the vision of the NCD to the commercial and economic development corridor. This approach is not meant to constrain opportunity in the commercial corridor. Instead, the Town seeks to identify economic development opportunities that support the historic preservation and family-character aspects of the vision of the adjacent NCD.

## Project Scope

The goal of the project is to create a Guide that sets forth an economic development vision for the West Rosemary Street corridor that is feasible from a market perspective and is consistent with the vision statements of the Northside NCD. The project seeks to resolve the tension between the goals of neighborhood conservation and economic development that are associated with the zoning overlays along the West Rosemary Street corridor by asking the question, "is it possible to develop a vibrant commercial and economic development corridor along West Rosemary Street that is consistent with the Vision Statements of the Northside Neighborhood Conservation District?" Understanding that there will be trade offs for both preservation and economic development policy areas,

The guidelines address the following:



West Rosemary Development Guide Process

- Local and Minority Business District: Vision for space and programs that support current and future local and minority-owned businesses along Rosemary Street.
- Cultural and Historic Preservation: Opportunities to celebrate and honor the historically African-American Northside neighborhood through cultural art.
- Innovation and Entrepreneurship: Space and programs that support current and future needs for innovation and entrepreneurship.
- Gradient of Development: Expectations for how the form of redevelopment along Rosemary Street will be compatible with the scale of existing neighborhoods.
- Infrastructure and Connectivity: Preferences for the inclusion of bike lanes, community broadband, vehicle and transit connectivity, and recreational spaces that enhance the public realm and improve accessibility.
- Redevelopment: Guidelines that support the preference for office and commercial uses and expectations for residential uses that would support the family character and affordable housing goals of the neighborhood and not contribute to the trend of student rentals.
- Parking: Solutions that support preferred development types and do not increase parking demand on the Northside neighborhood.

## Process

A unique factor in this project is that the topic of developing a vision for this corridor of Downtown has been under discussion for over six years. Therefore, the process included the input that has been given in the past as well as providing opportunities for people to offer new input. The graphic above outlines the process directly related to this Development Guide, from initial meetings through regulatory review.

## Future Use

The Guide will provide guidance for public, private and nonprofit actors to make decisions about this key Downtown corridor. For new development, the primary method of implementation is to be a reference for staff and applicants at relevant times, for relevant projects. The Guide will also provide a foundation for ongoing collaboration between the Town and the Northside community to address economic development and commercial development issues and opportunities, and provide guidance to Town staff, Town Advisory Boards/Commissions, and project applicants on expectations for the future economic growth of the area.



HARGRAVES  
COMMUNITY  
CENTER

ST JOSEPH CHRISTIAN  
METHODIST CHURCH &  
JACKSON CENTER

ROSEMARY  
VILLAGE

MAMA DIP'S  
KITCHEN

BREAD &  
BUTTER

BREADMAN'S

GREENBRIDGE  
CONDOMINIUMS

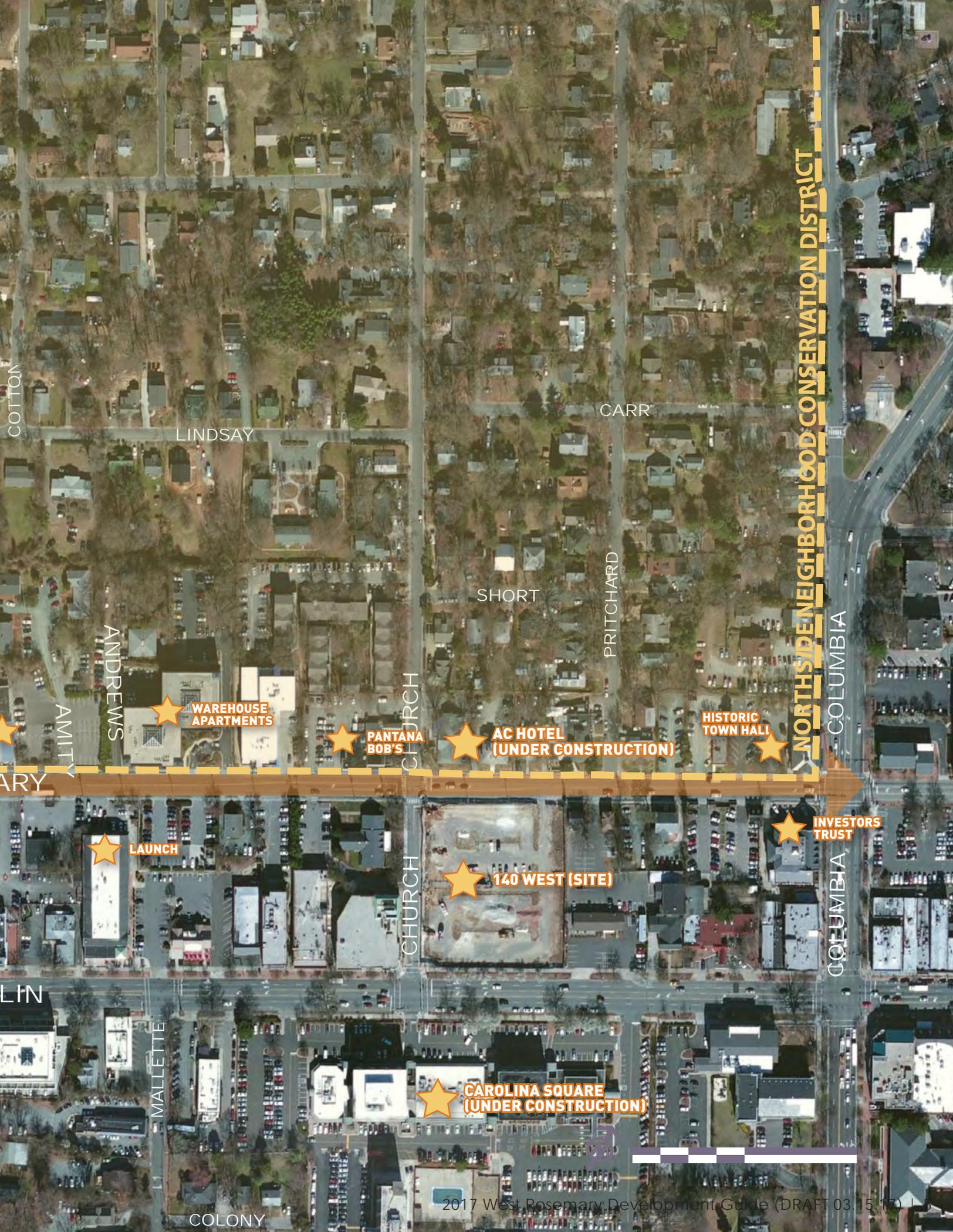
FIRST BAPTIST  
CHURCH

SHORTBREAD  
LOFTS (SITE)

EMPOWERMENT INC.

CHAPEL HILL - CARBORRO CITY LIMITS

# West Rosemary Today



**NORTHSIDE NEIGHBORHOOD CONSERVATION DISTRICT**

**WAREHOUSE APARTMENTS**

**PANTANA BOB'S**

**AC HOTEL (UNDER CONSTRUCTION)**

**HISTORIC TOWN HALL**

**INVESTORS TRUST**

**140 WEST (SITE)**

**CAROLINA SQUARE (UNDER CONSTRUCTION)**

COTTON

LINDSAY

CARR

SHORT

PRITCHARD

ANDREWS

CHURCH

COLUMBIA

AMITY

ARY

LIN

MALLETTE

COLUMBIA

COLONY

# Previous Studies

## Brief overview of past plans

The following studies and plans were referenced in the creation of this Development Guide.

- Design Guidelines: Downtown Chapel Hill (1998)
- Downtown Streetscape and Lighting Master Plan (2009)
- Northside and Pine Knolls Community Plans (2012)
- Northside Housing Market Action Plan (2013)
- Greenways Master Plan (2013)
- Rosemary Imagined (2013)
- Chapel Hill Bike Plan (2014)
- Downtown Imagined: Community Visions (2014)
- Northside Neighborhood Conservation District Plan (2004, ammend. 2014)
- Northside Neighborhood Initiative (2015)
- Affordable Housing Analysis Summary and Recommendations (2016)
- Chapel Hill Downtown 2020 Work Plan (2016)
- Town of Chapel Hill Cultural Arts Plan (2016)
- North-South Corridor Bus Rapid Transit (BRT) (2016)
- Chapel Hill Mobility & Connectivity Plan (Ongoing)



# Design Guidelines: Downtown Chapel Hill (1998)

Downtown Small Area Plan Work Group

## SUMMARY

These guidelines have been prepared to articulate what design objectives were expected as new development and redevelopment occurred Downtown. The guidelines were intended to provide guidance to both designers and citizens in the preparation, review, and approval of plans.

Key objectives included:

- Buildings should be designed to be compatible, in form and proportion, with neighboring buildings.
- Buildings or groups of buildings should include a variety of forms, materials, and colors, yet these elements should be composed to maintain a unified appearance.
- Buildings must include a richness of architectural detail to help define their scale.
- Buildings should extend to the back of the sidewalk.

For more information on Design Guidelines: Downtown Chapel Hill visit -

[www.townofchapelhill.org](http://www.townofchapelhill.org).



*Photo courtesy of - Design Guidelines:  
Downtown Chapel Hill, 1998*

# Chapel Hill Downtown Streetscape and Lighting Master Plan (2009)

Town of Chapel Hill (Report by Mikyoung Kim Design)

## SUMMARY

The Downtown Streetscape and Lighting Master Plan outlined a series of improvement projects for the downtown core. Specific focus along West Rosemary Street was given to the following:

- Sidewalks - looking for opportunities to expand the narrow and disconnected portions of sidewalk along West Rosemary.
- Lighting - providing lighting along both sides of the street in compliance with the Illuminating Engineering Society of North America (IESNA) standards.
- Planting - enhance the visual character of the street through additional plantings, with a long-term goal of acquiring easements on private property where the right-of-way is constrained to plant additional vegetation.
- Utilities - bury the overhead utility lines that are impeding successful street tree canopies and detracting from the visual aesthetics of the street.
- Nano parks - where opportunities are available within private development create small open space areas adjacent to the sidewalk.

Streetscape Improvement plan - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009





# Northside and Pine Knolls Community Plan (2012)

Town of Chapel Hill, Sustaining OurSelves Coalition, and the Northside and Pine Knolls Communities

## SUMMARY

During the summer and early fall of 2011, the Town of Chapel Hill, in collaboration with the Sustaining OurSelves Coalition and the Northside and Pine Knolls communities developed solutions to the impacts of incompatible development and student rental development on the neighborhoods.

For more information on the Northside and Pine Knolls Community Plan visit -

[www.townofchapelhill.org](http://www.townofchapelhill.org).

The Northside and Pine Knolls Community Plan set forth solutions to address the following Community Themes:

- Affordable Housing
- Cultural and Historic Preservation
- Enforcement
- Education and Outreach
- Parking
- Zoning



Photos courtesy of - Northside and Pine Knolls Community Plan, 2012

# Northside Housing Market Action Plan (2013)

Center for Community Self-Help, the Marian Cheek Jackson Center, the Northside Compass Group, and various community stakeholders

## SUMMARY

The Center for Community Self-Help, in collaboration with various community organizations called for action because they believed the Northside neighborhood stood at a crossroad due to student rental investment, Northside's history as a hub for local employee housing and African-American community withering away.

To advance the goals of the Chapel Hill 2020 Plan, to address the University and Town's workforce housing needs, or reach the full potential of the Northside, the Northside Housing Market Action Plan, outlined a comprehensive, neighborhood-driven community investment strategy that touches more than two hundred homes within five years, acting as a catalyst for further investment in the medium term and attaining the community's vision in the long term.

For more information on the Northside Housing Market Action Plan visit -

[www.townofchapelhill.org](http://www.townofchapelhill.org).



Photos courtesy of - Northside Housing Market Action Plan, 2013

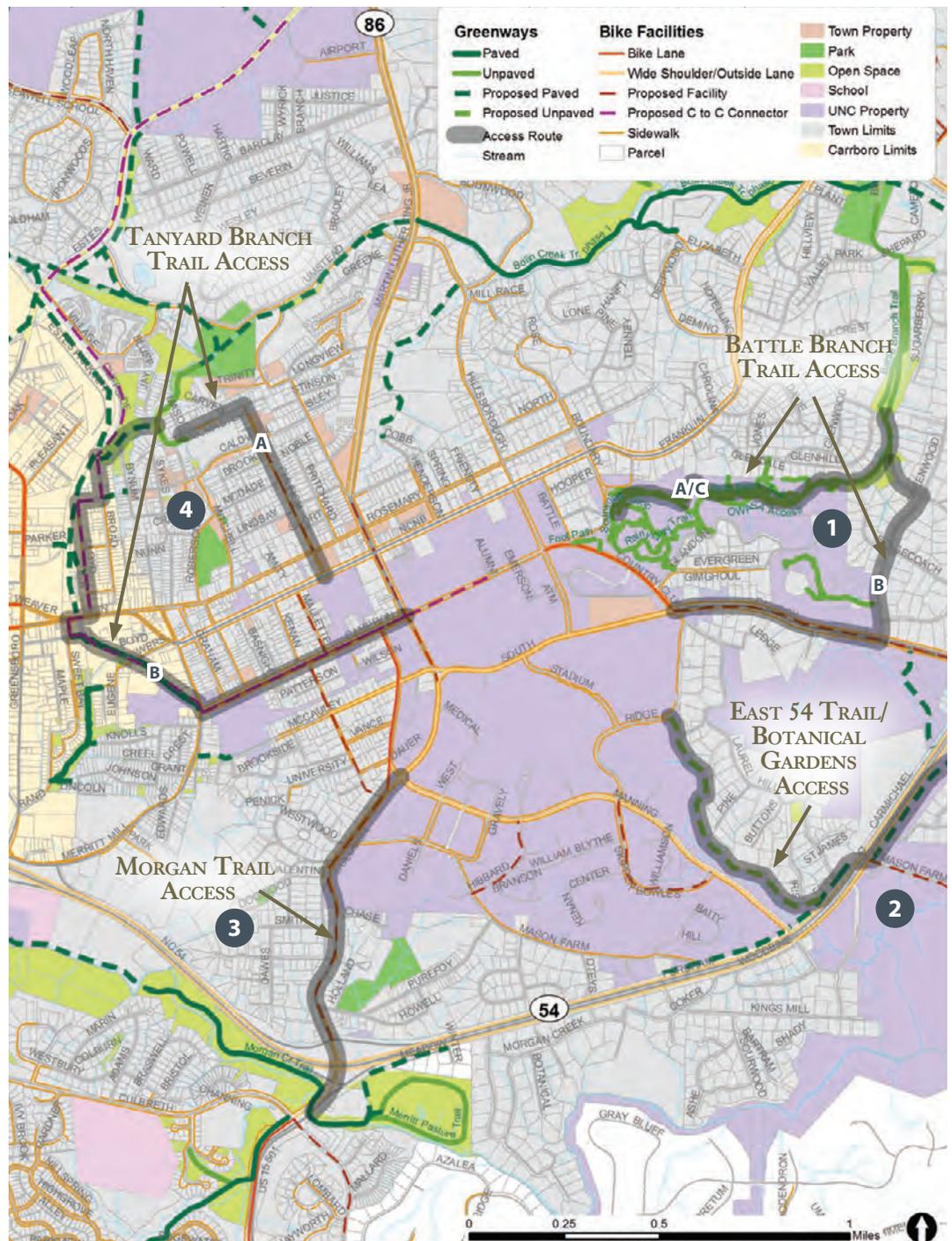


# Greenways Master Plan (2013)

Town of Chapel Hill

## SUMMARY

The Town has a short term plan to build a continuous paved trail from the Chapel Hill Community Center to the Northside neighborhood near the downtown. This would be accomplished by joining the Bolin Creek and Tanyard Branch trails. The Tanyard Branch Trail is complete from Cotton Street to Jay Street. The Bolin Creek Trail is complete from the Community Center to Martin Luther King Jr. Blvd. The completion of Phase 3A will complete all but a .5 mile segment of the trail system. We intend to complete design and permitting of the final segment in early 2016 and will be in a position to complete the Bolin Creek/Tanyard connection as soon as funds are available.



# Rosemary Imagined (2013)

Town of Chapel Hill

## SUMMARY

From May 2013 to August 2014, community members, visitors, and residents were asked to share their thoughts about the future of Rosemary Street. The input and feedback were used to develop the draft Rosemary Street Vision and Implementation Plan.

In August of 2014, the Rosemary Imagined planning process was expanded to include all of downtown and was renamed Downtown Imagined.

For more information on Rosemary Imagined visit - [www.townofchapelhill.org](http://www.townofchapelhill.org).

Photos courtesy of - Rosemary Imagined, 2013





# Chapel Hill Bike Plan (2014)

Town of Chapel Hill

## SUMMARY

The purpose of Chapel Hill Bike Plan is to provide the Town with a set of prioritized infrastructure improvements, policies and program recommendations that will guide Town decisions and investments for the future.

The goals of the Bike Plan are to:

1. Improve the safety of bicycling for all types of riders. Safety was the number one concern raised during the development of this plan. Many suggested that safety concerns prevented people from bicycling in Town.
2. Foster the development of a culture where bicycling is an accepted and viable mode choice in Chapel Hill. Though some people regularly bicycle in Town today, it is still not considered to be the most reasonable or attractive mode of transportation by many people. An awareness of the bicycle as an efficient and good way to get around Town will help make it a more regular mode of community travel.
3. Develop a connected network of bicycle facilities in coordination with greenways throughout Chapel Hill. Today, the low level of street connectivity in Chapel Hill is a major barrier to making bicycling a convenient choice, as bicyclists must travel on high stress arterials with inadequate bike accommodations for portions of a trip.
4. Increase bicycle use for all types of trips. Everyday, people in Chapel Hill make different types of trips like commuting to work, running errands, or going shopping. Many of these trips are short enough to be made by bicycle.



ABOVE: Image from the NACTO Urban Bikeway Design Guide showing an 8 inch solid marking separating the bike lane from the motor vehicle travel lane.

“A bike lane should be delineated from the motor vehicle travel lanes with a 150-mm (6-inch) solid white line. Some jurisdictions have used a 200-mm (8-inch) line for added distinction.”

SOURCE: AASHTO. (1999). *GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES*.

Image courtesy of - Chapel Hill Bike Plan, 2014

# Downtown Imagined: Community Visions (2014)

Town of Chapel Hill

## SUMMARY

Downtown Imagined was intended to provide visions and ideas for the future of Downtown Chapel Hill. Within this plan, the visions were organized by the following themes:

- Affordable and Multi-family Housing
- Space for Youth and Teens
- Desired Downtown Commerce
- Entrepreneurial & Innovation Hub
- Collectivity and Mobility
- Managed Traffic Congestion
- Bicycle Infrastructure
- Design District
- Available Downtown Parking
- Addressing Infrastructure Gaps
- Cultural and Arts Space
- Buffers Between Neighborhood and Commercial Areas
- Historical and Cultural Center
- Development and Redevelopment
- Gathering and Green Spaces
- Partnership and Collaboration

For more information on Downtown Imagined visit - [www.townofchapelhill.org/downtown](http://www.townofchapelhill.org/downtown).

Photos courtesy of - *Downtown Imagined: Community Visions, 2014*





# Northside Neighborhood Initiative (2015)

Town of Chapel Hill

## VISION

Since Spring 2015, the Northside Neighborhood Initiative (NNI), a partnership among UNC-Chapel Hill, Self-Help, the Jackson Center, and the Town of Chapel Hill, pursued a shared vision for a vibrant, diverse, family-friendly neighborhood that includes elders, students, families, and others in dynamic inter-generational relationships and honors the history of Northside.

The vision has been conveyed through decades of neighborhood and organizational leadership, hundreds of oral histories, porch conversations, and recent planning efforts in collaboration with neighborhood, Town, and UNC leaders.

## GOALS

The NNI was designed to balance housing types in the neighborhood, in order to preserve the future of Northside as a community that serves homeowners and renters from a variety of backgrounds with an emphasis on people who work in Chapel Hill and Carrboro, by pursuing the following specific goals:

1. Helping long-term residents who want to stay in Northside remain in their homes;
2. Attracting new residents - a balance of working families, seniors, and students from diverse backgrounds; and
3. Increasing the availability of housing and financing options for neighborhood properties.

For more information on Northside Neighborhood Initiative visit - [www.townofchapelhill.org](http://www.townofchapelhill.org).



Photo courtesy of - [townofchapelhill.org](http://townofchapelhill.org)

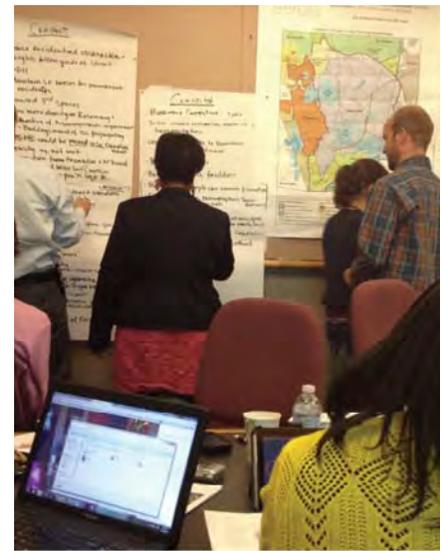
# Northside Neighborhood Conservation District Plan (2004, Ammended 2012)

Town of Chapel Hill

## SUMMARY

The Northside Neighborhood Conservation District (NCD) established special regulations especially designed for and intended to help preserve the character of a particular, older residential neighborhood. The special regulations included:

- Height limits
- Floor-area ratios (FAR)
- Bedroom-to-bathroom ratios
- Limitations on front yard parking
- Specific requirements applying only to single-family development and single-family dwellings with accessory apartments.





# Affordable Housing Analysis Summary and Recommendations (2016)

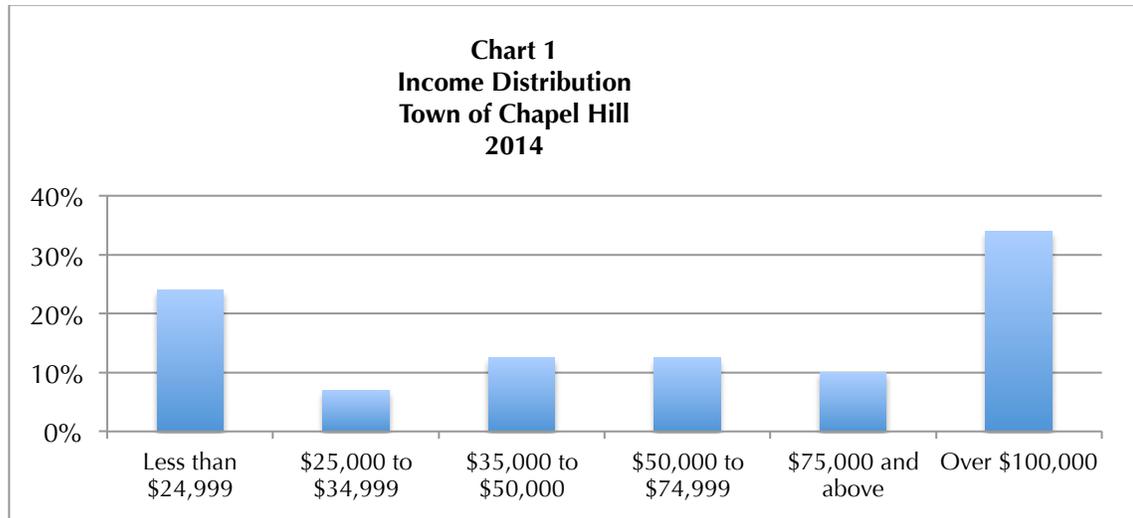
Town of Chapel Hill

## SUMMARY

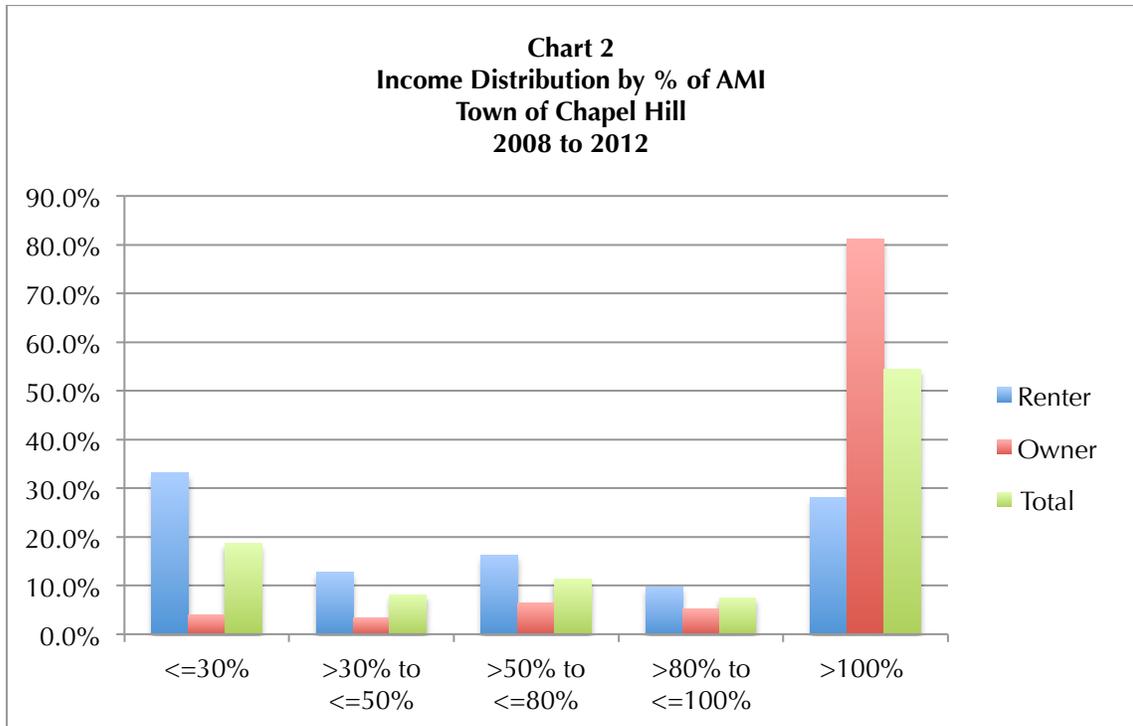
The report lays out the best path forward to provide for the affordable housing needs of the community and maximize the Town's affordable housing resources.

The policy recommendations for the Town's affordable housing program are:

1. Opportunity-driven program
2. Income targeting and tenure
3. Current resources for affordable housing
4. New revenue sources and land use tools for affordable housing
5. Strategic and capital plan
6. Site-specific recommendations and next steps



Sources: ACS 5-year estimates, DRA.



Sources: HUD Comprehensive Housing Affordability Strategy (CHAS) data for 2008-2012; DRA.

Images courtesy of - Affordable Housing Analysis Summary and Recommendations, 2016

# Chapel Hill Downtown 2020 Work Plan (2016)

Town of Chapel Hill

The Downtown 2020 Work Plan transforms ideas into action starting in FY 17 and working toward FY 20.

		Phase One (FY 17)	Phase Two (FY 18)
 <p><b>Development Patterns</b></p>	Harmonize new buildings with the old, so that mid-rises do not tower over houses. Encourage a greater variety of housing and mixed-use courtyard development with room for start-ups.	<ul style="list-style-type: none"> <li>Create additional <b>guidelines and regulations for Rosemary Street</b> regarding commercial development and land use that are consistent with the vision statement of the Northside Neighborhood Conservation District.</li> </ul>	<ul style="list-style-type: none"> <li>Work collaboratively with the University to develop a policy for developing and continuously assessing <b>future student housing development</b> both on and off campus that prioritizes <b>student safety</b> and <b>protects the historic neighborhoods</b> that surround the University.</li> <li>Create <b>Design Guidelines</b> for Downtown that address gradient of development and standards for infill and courtyard development.</li> </ul>
 <p><b>Access</b></p>	Improve access for the physically and visually-impaired, parking, way-finding, biking, pedestrian crossings, through-block connections, and traffic flow past Columbia Street.	<ul style="list-style-type: none"> <li>Conduct a <b>holistic study of downtown parking and circulation</b> that informs decision making about street infrastructure.</li> <li>Identify alternative models for <b>parking management</b> and metering.</li> <li><b>Increase lighting on alleyways.</b></li> </ul>	<ul style="list-style-type: none"> <li><b>Restripe Franklin and Rosemary</b> to in accordance with the downtown parking and circulation study.</li> <li>Construct improvements to <b>pedestrian crossings</b>, other traffic calming measures.</li> <li>Consider <b>moving bus transfer</b> stops away from Columbia Street.</li> </ul>
 <p><b>Variety</b></p>	A trip downtown should involve surprise and delight—the exploration and discovery of new places and the sound and motion of public performance.	<ul style="list-style-type: none"> <li>Increase exploration on informal and formal pathways with <b>creative pedestrian level wayfinding signage.</b></li> <li>Recruit <b>artistic programming and small events</b> to increase vibrancy and year round activity.</li> </ul>	<ul style="list-style-type: none"> <li>Consistent program of <b>artistic programming and small events</b> to increase vibrancy and year round activity.</li> </ul>
 <p><b>Public Realm</b></p>	Create active public gathering spaces and make the sidewalks a better place to sit and socialize.	<ul style="list-style-type: none"> <li>Research, test and evaluate ideas for <b>activating public realm and green space</b> (ITS Plaza, 140 West, and others).</li> <li>Reduce regulatory barriers to <b>sidewalk dining</b> and support <b>façade improvements</b> that activate and beautify the street.</li> </ul>	<ul style="list-style-type: none"> <li>Evaluate and remove excess hardware from <b>sitting areas</b> between street and sidewalk.</li> <li>Consider removal of regulatory barriers to small business and entrepreneurial start-up activity.</li> </ul>
 <p><b>Economic Development</b></p>	Chapel Hill is the home of choice for innovators, start-ups and entrepreneurs.	<ul style="list-style-type: none"> <li>Identify <b>co-working and Stage II space</b> for companies.</li> <li>Increase the economics of downtown through <b>strategic drivers.</b></li> </ul>	<ul style="list-style-type: none"> <li>Identify <b>co-working and Stage II space</b> for companies.</li> <li>Increase the economics of downtown through <b>strategic drivers.</b></li> </ul>

6-2016 // Contact Rae Buckley, 919-969-5007, rbuckley@townofchapelhill.org



# Town of Chapel Hill Cultural Arts Plan (2016)

Town of Chapel Hill, Cultural Arts Division of Parks & Recreation Department

## SUMMARY

Cultural Arts Plan Goals:

1. To **Engage, Educate** and **Empower** people of all ages by expanding access to a variety of arts and cultural opportunities in community settings.
2. To contribute to the tourism and **Economic** vibrancy of Chapel Hill.
3. To **Enrich** the physical Environment by integrating public art throughout Chapel Hill.
4. To **Expand** infrastructure - facilities, staffing, marketing, and budget - to support a thriving cultural community.



Within the plan itself and during the public input process the Rosemary corridor and the Northside neighborhood were mentioned specifically:

- Continue to update and expand inventories of arts and cultural programming spaces along Franklin and Rosemary Streets; e.g. Town, UNC, churches, community, etc.
- Rosemary street could be developed as a creative hub – support for non-profit art start-ups
- Banners across Rosemary
- Create a historical and cultural center near Northside



Above: Dancers perform in the plaza of 140 West  
Below: Hargraves Legacy Murals and Paintings, 2004 by David Wilson, which presents a history of the Hargrave Center's community and political leaders, its central role in the Northside neighborhood and its significance as the first social and recreational facility for African-Americans in Chapel Hill.  
Images courtesy of - Chapel Hill Cultural Arts Plan, 2016

# North-South Corridor Bus Rapid Transit (BRT) (2016)

Town of Chapel Hill

## SUMMARY

“Chapel Hill Transit is conducting the North-South Corridor Alternatives Analysis, “North-South corridor Study” for short, to evaluate the best methods for moving more residents and commuters along the Martin Luther King, Jr. Boulevard and 15-501 South corridor. An AA is a detailed study of all reasonable transportation alternatives within a corridor that addresses identified transportation problems. The study area runs from UNC North Campus (a northern terminus) to the Southern Village (the southern terminus) and points in between.”

November 21, 2016: The Federal Transit Administration (FTA) notified Chapel Hill Transit that the North-South Corridor Bus Rapid Transit (BRT) Project was formally accepted into the FTA’s Small Starts Project Development program. This

acceptance into the FTA program will mean that the BRT project can move forward into further engineering analysis and environmental impact review.

The North-South Corridor Study itself is the first step in a multi-phased federal process. The chart below shows key process milestones that would need to be accomplished after completion of this study.

A Locally Preferred Alternative (LPA) has been selected that shows the BRT route running along Columbia Street perpendicular to the corridor. The LPA was approved by council in April.

For more information on the North-South Corridor Study visit - [www.nscstudy.org](http://www.nscstudy.org)





# Chapel Hill Mobility and Connectivity Plan (Ongoing)

Town of Chapel Hill

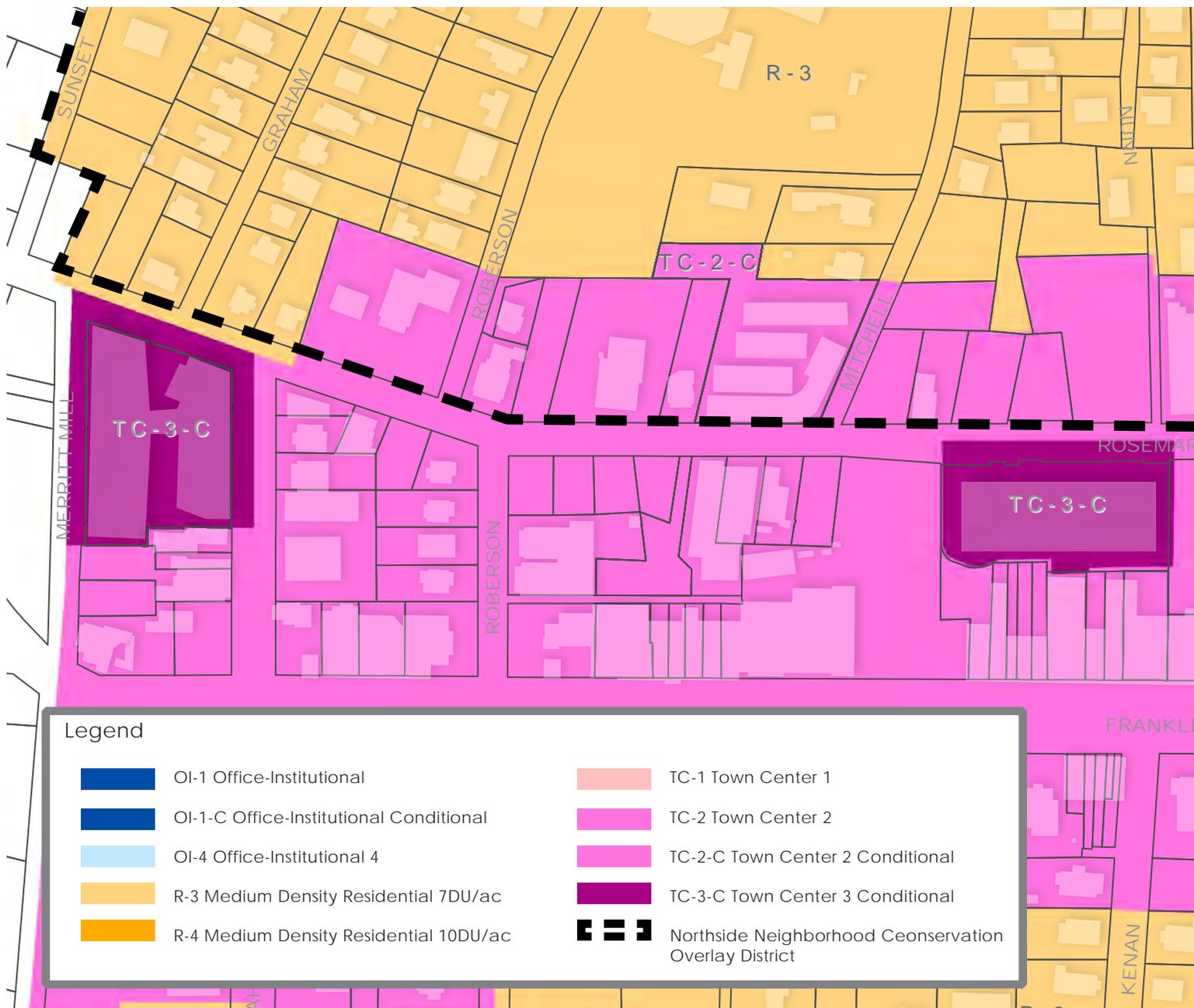
## SUMMARY

The Town of Chapel Hill is currently drafting a Mobility and Connectivity Plan that will recommend connections to significant destinations, close gaps in walkability, and encourage healthier and more active behavior in residents and visitors.

For more information on the Chapel Hill Mobility and Connectivity Plan visit - [www.townofchapelhill.org/residents/transportation/bicycle-and-pedestrian/chapel-hill-mobility-and-connectivity-plan](http://www.townofchapelhill.org/residents/transportation/bicycle-and-pedestrian/chapel-hill-mobility-and-connectivity-plan).

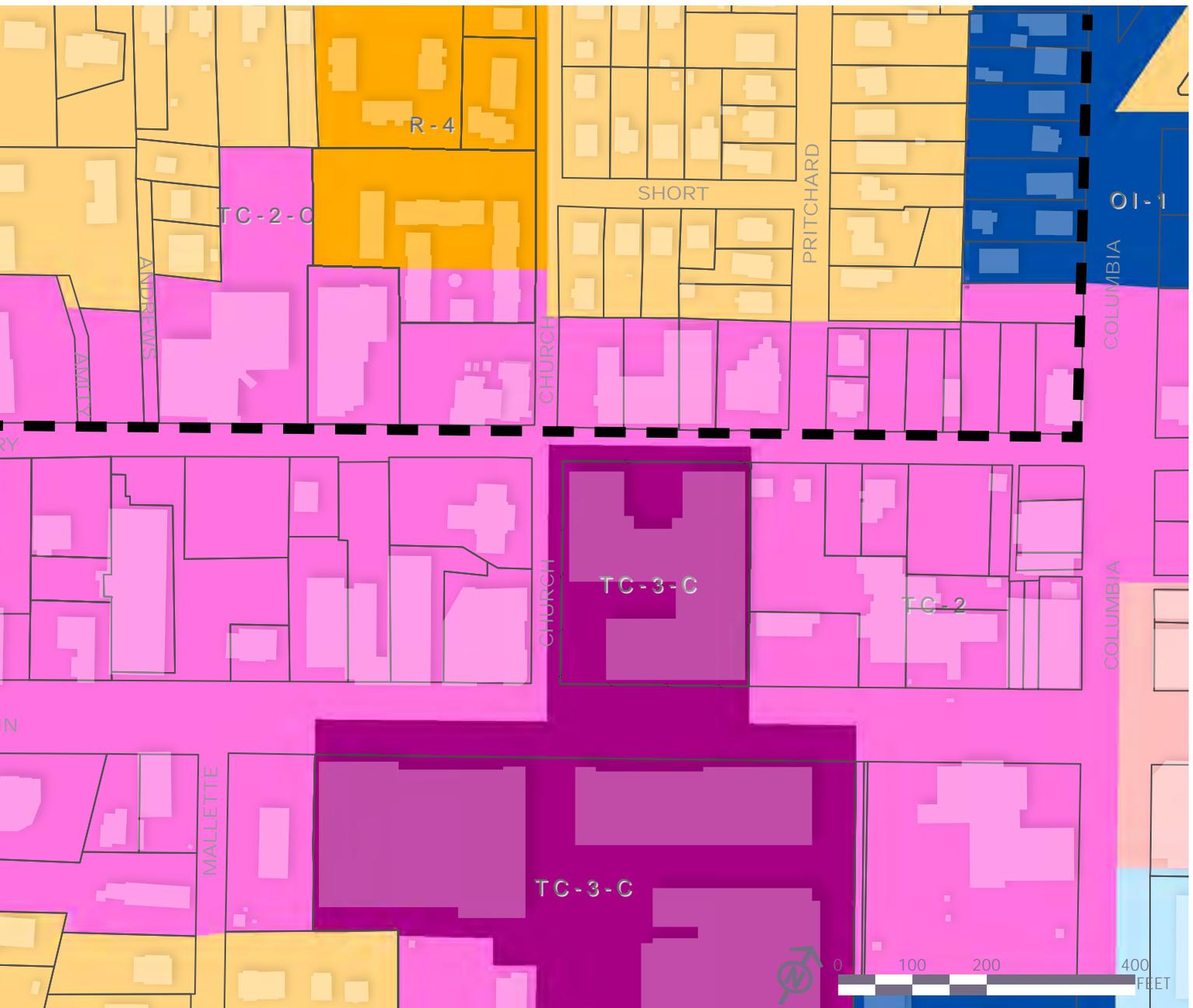
# Zoning

## Current Downtown Zoning





The West Rosemary corridor is primarily zoned as Town Center 2 and Town Centre 3 with portions of Residential 3 and even 4 covering the Northside neighborhood directly adjacent to the corridor (see more detailed explanations of each zoning type on the following page). The Northside Neighborhood Conservation Overlay District (NCD), enacted in 2004, affects building standards along the entire north side of the street (see more details regarding the NCD on the following pages).



# West Rosemary Zoning

## Chapel Hill Code of Ordinance

### Town Center

The Town Center districts are intended to provide for the development of the commercial, service, and social center of Chapel Hill while maintaining its character, its pedestrian-oriented scale, and its nature as a concentration of business, administrative, financial, governmental, and support functions serving the community; and to encourage further residential development in the central area of Chapel Hill. The zoning regulations for the TC districts are designed to achieve the following objectives:

- Buildings designed to provide visual interest
- Creation of enjoyable, human-scale spaces.
- Buildings blend with the natural terrain
- Designs are compatible with the neighboring area.
- Designs strive for creativity in form and space
- Projects include visual variety and a strong sense of unity
- Projects include a high level of architectural detailing to help maintain a sense of scale



#### TC-2 & TC-2-C Town Center 2\*

- No minimum lot size, maximum density, or setback
- Maximum building height of 44' (approx. 4 stories) at the street level
- Maximum core height of 90' (approx. 9 stories) with a 1:1 transitional height plane from the street and interior and 1:1.7 from the solar setback

#### TC-3-C Town Center 3

- No minimum lot size, maximum density, or setback
- Maximum building height of 44' (approx. 4 stories) at the street level
- Maximum core height of 120' (approx. 12 stories) with a 1:1 transitional height plane from the street and interior and 1:1.7 from the solar setback

\* All zoning shown with an asterisk is affected by the Northside Neighborhood Conservation District (see the following spread)



# Office-Institutional

The office/institutional-1 (OI-1) district is intended to provide for low-intensity office and institutional development and, where appropriate, to serve as a transition between residential zoning districts and high-intensity nonresidential zoning districts.



## ■ OI-1 & OI-1-C Office-Institutional\*

- Minimum lot size of 5,500 sf
- Maximum Density of 10 DU / ac
- Maximum building height of 29' (approx. 3 stories) at the street level
- Maximum core height of 60' (approx. 6 stories) with a 1:2 transitional height plane from the perimeter
- Street setback of 24' / Interior setback of 8' / Solar setback of 11' (north lot lines only)
- Maximum Floor Area Ratio of .264

# Residential

The residential districts are intended to provide for residential development of appropriate intensities consonant with the suitability of land, availability of public services, accessibility to major activity centers and transportation systems, and compatibility with surrounding development.



## ■ R-3 Medium Density Residential\*

- Minimum lot size of 5,500 sf
- Maximum Density of 7 DU / ac (typically single-family or duplex)
- Maximum building height of 29' (approx. 3 stories) at the street level
- Maximum core height of 60' (approx. 6 stories) with a 1:2 transitional height plane from the perimeter
- Street setback of 24' / Interior setback of 8' / Solar setback of 11' (north lot lines only)

## ■ R-4 Medium Density Residential\*

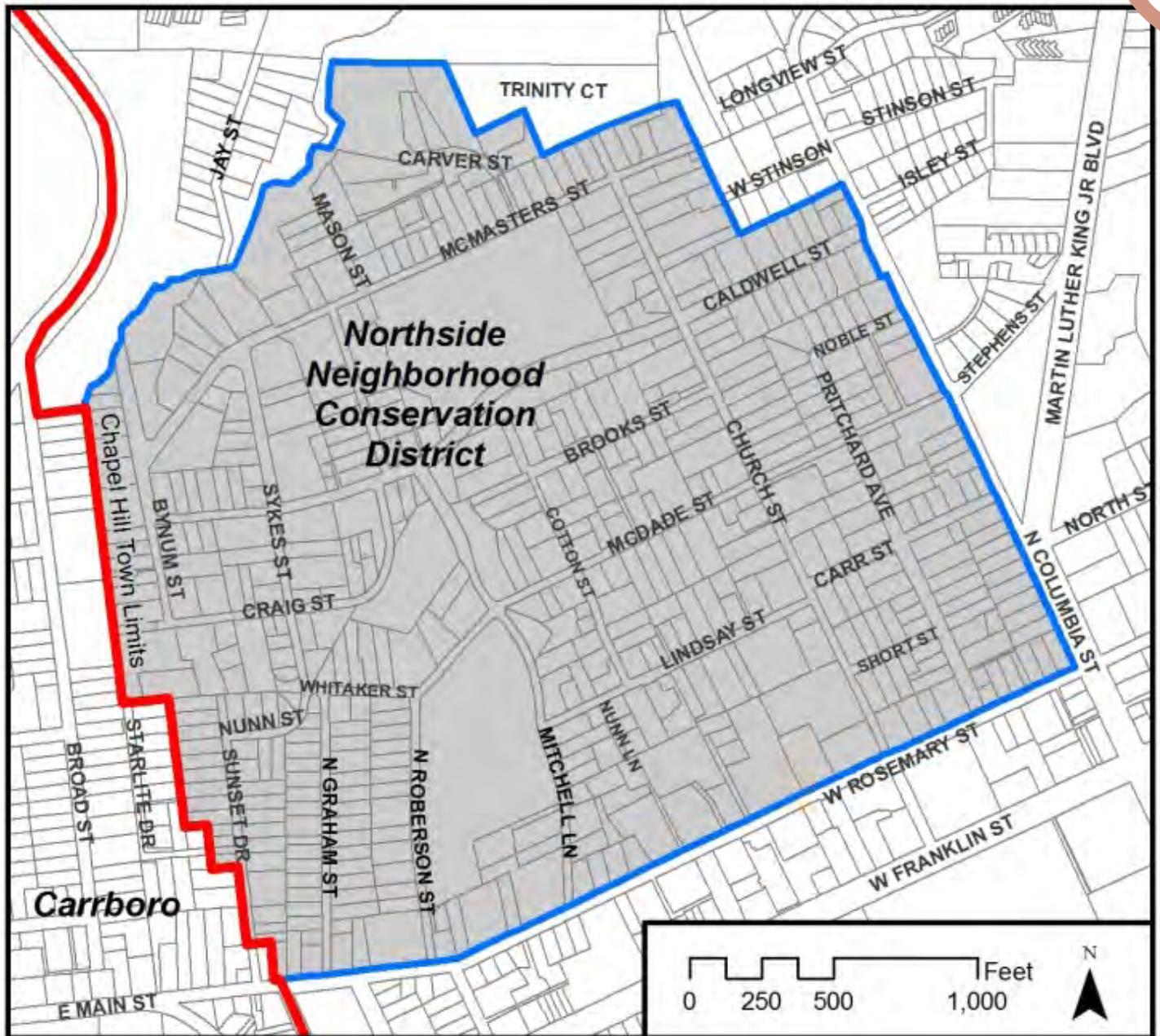
- Minimum lot size of 5,500 sf
- Maximum Density of 10 DU / ac (typically single-family, duplex, and multifamily)
- Maximum building height of 34' (approx. 3 stories) at the street level
- Maximum core height of 60' (approx. 6 stories) with a 1:2 transitional height plane from the perimeter
- Street setback of 22' / Interior setback of 8' / Solar setback of 9' (north lot lines only)

# Conservation District

## Northside Neighborhood

### Vision Statement

- Preserve the history, charm and composition of a proud and **historic community**.
- Promote and protect the **diversity** and family character of the neighborhood.
- Protect the **family atmosphere** and ensure that all future development is comparable with the majority of the neighborhood in scale and function.
- As properties change ownership, make certain that families seeking **homeownership** have opportunities to buy a home and can afford to live in this community.
- Promote more **affordable** homeownership opportunities for low and moderate income families and households.
- Nurture an environment that promotes **community interaction** and fosters a safe and **proud neighborhood**.



## Special Northside NCD Design Standards

Compared to the standard Chapel Hill Zoning Ordinance (see previous spread)

	 R-3 Residential	 R-4 Residential	 O-I Office-Institutional	 TC-2 Town Center 2
Maximum building height at street level	29' 20'	34' 20'	29'	44' 40' (30' if next to residential)
Maximum core height	60' 26'	60' 26'	60' 35'	90' 50'



# Community Benefits



The Community Benefits section outlines some solutions to community-based issues. The notion being that with new development the neighborhood is seeing increased pressure to change what makes it unique. These benefits can be achieved through a number of different approaches, and from a variety of angles.

Items covered within this section include:

- Development cost related to density
- Local and minority-owned business
- Affordable Housing
- Homeownership
- Cultural identity

The Northside NCD Vision from the previous page remained a constant reference point for the development of this report and reinforced the positive aspects to the community vision. It was equally important for these guidelines to react to community input which explicitly held forth what the community did not want to see:

- More student rental projects
- Developments with unchecked heights well above what is outlined in the NCD
- No thought for transitions to the neighborhood
- No spaces for community-based businesses
- Increased traffic and congestion on neighborhood streets
- Loss of community culture

It is expected that all new development along West Rosemary will include some form of community benefits as outline in the following section.

# Market Analysis

## Development Cost & Density

The following table shows the discrepancy between the types of land use desired by the community (low density, retail) verses what the forecasted market demand is projecting (multi-family residential, condominiums). It also shows the approximated land value per acres for each land use type, and the minimum required density (number of floors) and ideal location along the West Rosemary corridor to make the development solvent.

Building Type	Closest In li Community Vision
Single Family 	✓
Townhouse 	✓ ✓ ✓
Retail 	✓ ✓ ✓
Multi-Family Apartments 	✓
Mid-Rise Office 	✓ ✓
High-Rise Office 	
High-Rise Condo 	
Hotel 	



<b>ne with:</b> Market Demand	<b>Land Value</b> (\$/Acre)	<b>Density</b> (Floors)	<b>Location</b>
✓	\$500k	1	Neighborhood Adjacent
✓	\$1M	2	Neighborhood Adjacent
✓	--	--	Only at Corners
✓ ✓ ✓	\$1.5M	4	Along Rosemary (lg. lot)
✓ ✓	\$2M	4	Along Rosemary (lg. lot)
✓ ✓	\$2.5M	8	Along Rosemary (lg. lot)
✓ ✓ ✓	\$3.5M	8	Rosemary Corner Lot
✓ ✓	\$4M	4-8	Rosemary Corner Lot

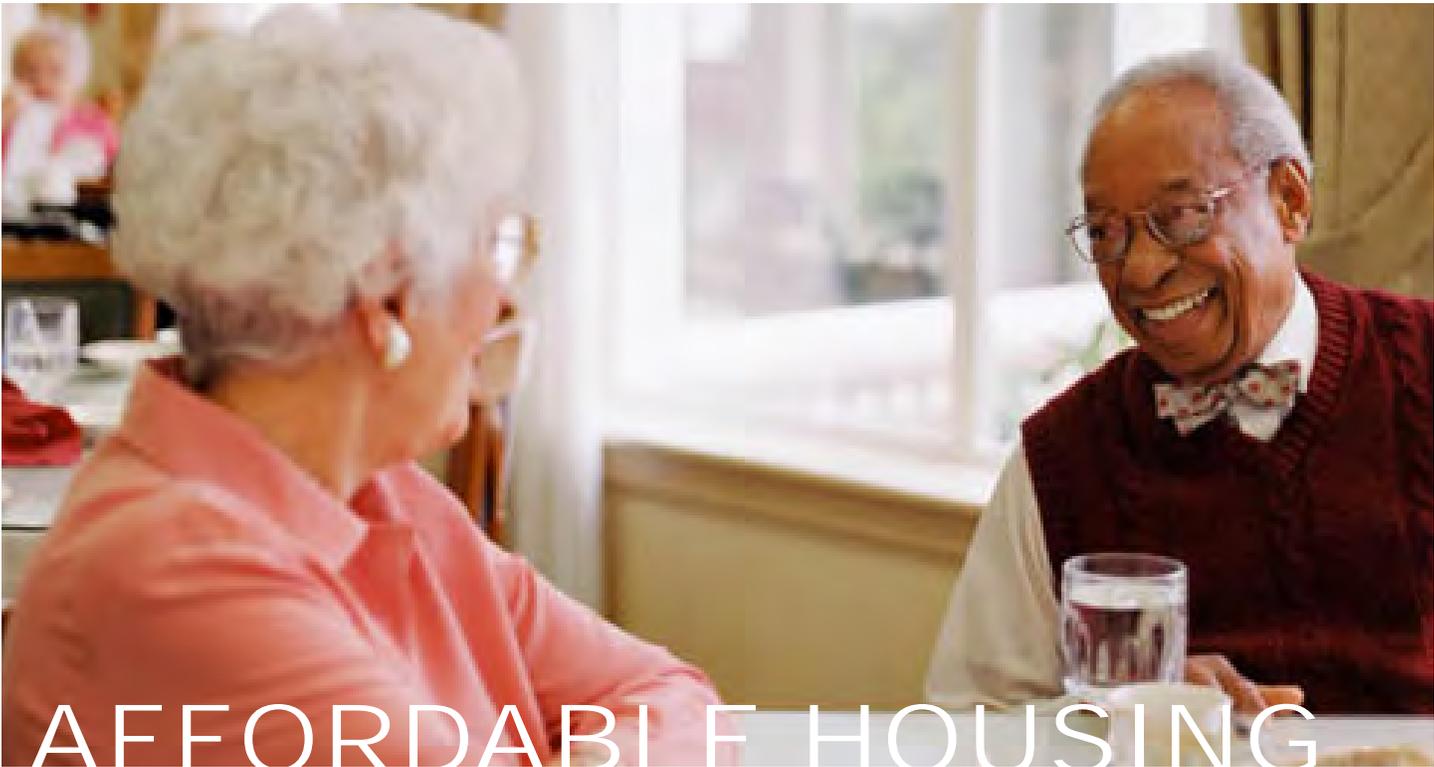
# Community Benefits

## Development Cost & Density





In response to public feedback it became clear that there were four categories related to development that dealt more with community benefits and less with the built form. The overarching concern was how to maintain the strength and culture of the Northside Neighborhood by encouraging local businesses to thrive and grow, to provide affordable housing options (especially rental) within new developments, to maintain affordable owner-occupied housing, and to highlight the cultural identity of the place.



# Community Benefits

Local and Minority-Owned Business

Ensure locally  
and minority-  
owned businesses  
are welcome &  
encouraged

Images courtesy of - Midway Kitchen (left) and 1789 Venture Lab (right)



Examples of small business training programs and shared office space



According to market research, it appears that the expected rental rate for commercial space along West Rosemary would be somewhere in the mid-\$20s per square foot range at corner locations. Based on feedback from business-owners in the community these rental rates exceed what they are willing or able to pay. To get lower rates for the community there would have to be a tax break, tax increment financing (TIF) incentive dollars, or other similar programs.

---

## provide commercial tax incentives

Consider grant programs that are aimed at small business, such as those that assist with tenant improvements.

---

## create shared retail or office space

By providing incentives for the developer on the real estate side the developer is then able to pass along those savings to their retail tenants. The idea is to take a larger “market hall” space (10,000 SF for example) at \$22/sf (\$18k/month), lose some space to internal corridors/seating, and take the remaining 8,000 SF and cut it into small stalls 10 x 10 or so which then only cost around \$300/month to rent. There will be higher shared operating costs of around \$150/mo, which would require finding a good experienced operator.

The same model can be applied to a shared office space. In a co-working model a larger space is divided into desks or small offices with a shared front desk/administrative space/conference room(s)/break and restrooms.

---

## regulate for small-scale spaces

By maintaining the small-scale character of a retail district through zoning regulations, large-scale retailers will be deterred. Encouraging smaller scale retail spaces can offer opportunities for smaller, local businesses. Typically 1,000 square feet or less.

# Community Benefits

Affordable housing



Include affordable housing in all new developments



## set affordable housing minimums for developers

Continue to enforce affordable housing minimums that are already covered by the existing Inclusionary Zoning ordinances.

## provide density bonuses

The Town has existing inclusionary zoning regulations which enable density bonuses to aid developers in supporting affordable housing requirements. The key will be in setting height limitations that give market value to added height. A density bonus in the West Rosemary corridor could have real value if base height is limited to four stories or less on the north side of the street.

## initiate a land write down

The Town could buy land and sell it below cost under the stipulation that the developer provide some type of community benefit - in this case designated locally-owned commercial space.

## public financing of infrastructure

Possibly public financing of structured parking decks and/or public investment in streetscape, etc. that could serve to lower the overall development cost to the developer in exchange for community benefits.

## encourage public/private development

Proactive assembly of a key development opportunity in partnership with a private or non-profit developer who would apply for tax credits and build an affordable housing development.

## establish tax incentive policies

Taxes on a multi-family apartment complex deal are running around \$2k/unit for the area, at an average of 10 years that equates to around a \$30-40k/unit. Exploring ways under North Carolina law to minimize these taxes could be a powerful incentive.

## explore new resources for creating affordable housing

Look into the latest models to create affordable housing.

## consider establishing a fund for large development projects

Create a fund to help developments of a certain size achieve larger affordable housing percentages.

## evaluate existing policies that could be barriers to affordable housing

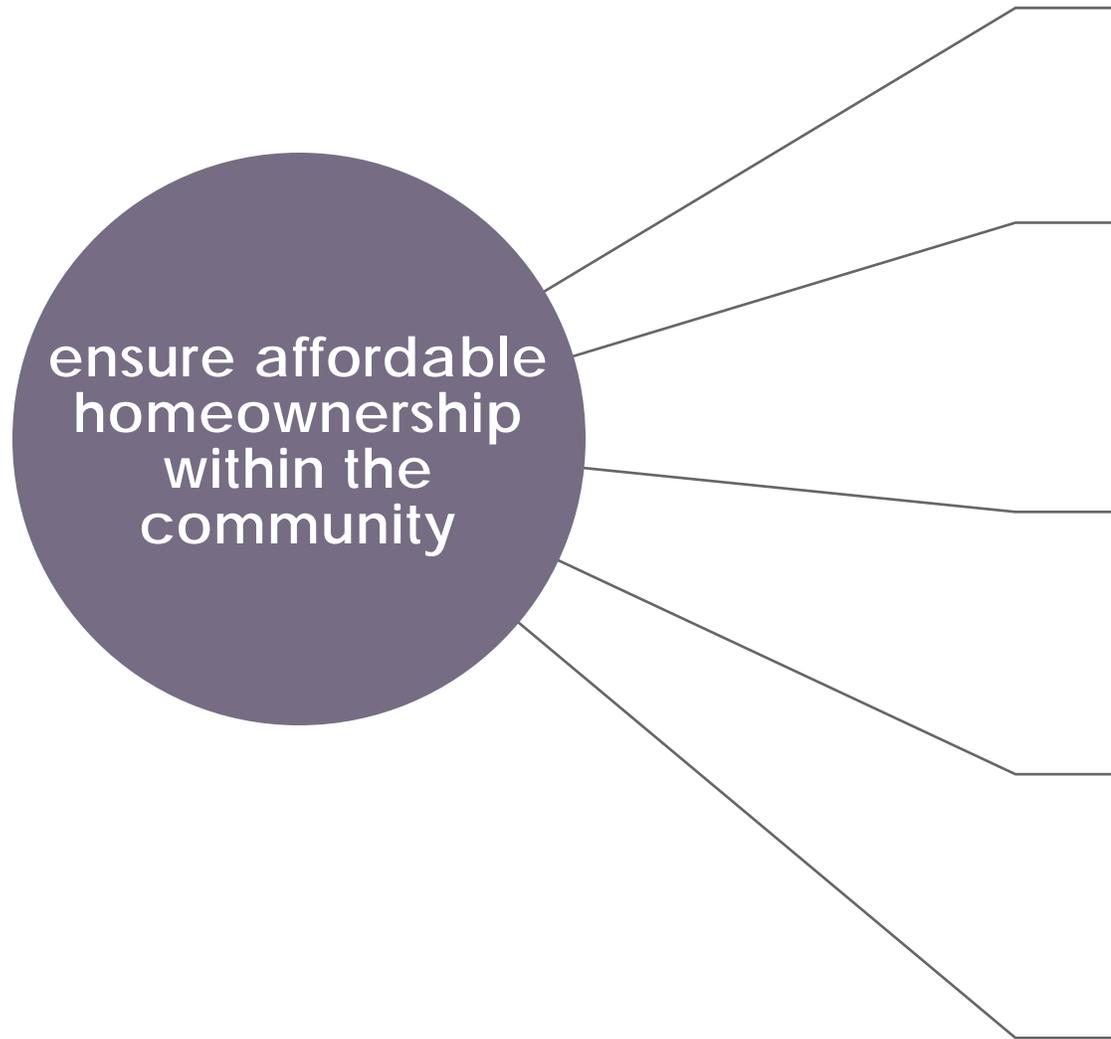
Consider refinements to policies that hinder the creation of affordable housing.

## evaluate incentives to encourage private sector partnerships

Create incentives to help successfully establish private partnerships to create new affordable housing.

# Community Benefits

## Homeownership





---

## encourage owner occupancy through tax incentives

Maintain stability and affordability through grants to assist owner occupants with raising property taxes.

---

## continue to use the community land trust model

A Community Land Trust (CLT) purchases property and holds it in perpetuity to be affordably sold to low-income families. If and when the time comes for the homeowner to sell, a buy-back clause allows for the CLT to repurchase the land at a reasonable price.

---

## create down-payment assistance programs

Creation of a formalized program to assist potential qualified homeowners who may have the ability to make a monthly mortgage payment but not the means to have a substantial down-payment for the purchase of a home.

---

## sponsor the creation of new homes

Sponsor new homes through Habitat for Humanity and the "Neighbors of Northside" initiative - using students, downtown residents, and business owners to help in the building process.

---

## utilize Town-owned land to develop new homes

Consider the inventory of Town-owned property for potential new home development.

# Community Benefits

Cultural identity

Ensure the preservation of the Northside neighborhood/  
Potter's Field identity



Northside Gateway/Memorial at the corner of Roberson Street



## develop a community benefits agreement

A written agreement could be developed that spells out how community benefits of local business, affordable housing, and/or priorities from the Cultural Resources Plan could be a requirement of new development approvals.

## perform a cultural resources plan

This could include identification of historic and cultural physical features, ideas for new public space and public art, new cultural programming, communication measures, and community cultural priorities.

## encourage community-based public art

The integration of public art is an opportunity to meld public art with the daily experience of traversing the Downtown District. The Streetscape Master Plan proposes installation of public art in several venues: as unique, site specific installations; integrated with walking surfaces and streetscape elements; or as district-wide temporal installations that establish a widespread visual marker throughout Downtown. - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009.

Public art should focus on physical place-making, municipal and university asset-sharing, branding, perception-changing events, and a system of support. - Cultural Arts Plan, 2016

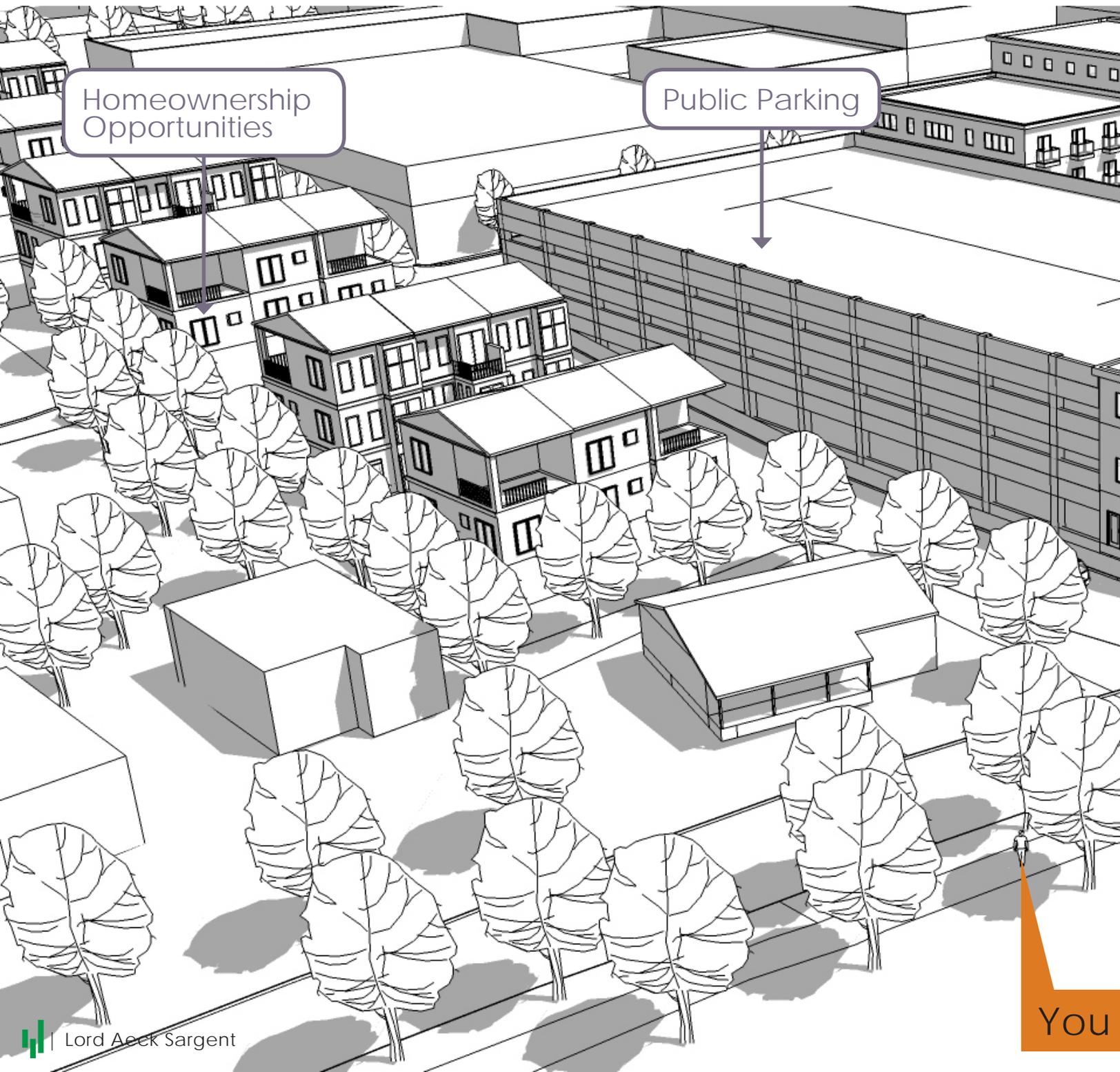
The proposed pocket parks provide opportunity for larger installations of interpretive art.

## tie wayfinding with cultural identity

Use proposed wayfinding signage to also help highlight and celebrate the community character and identity. Tie this to both a future Cultural Resources Plan and the 2016 Cultural Arts Plan.

# Community Benefits

## Neighborhood Perspective



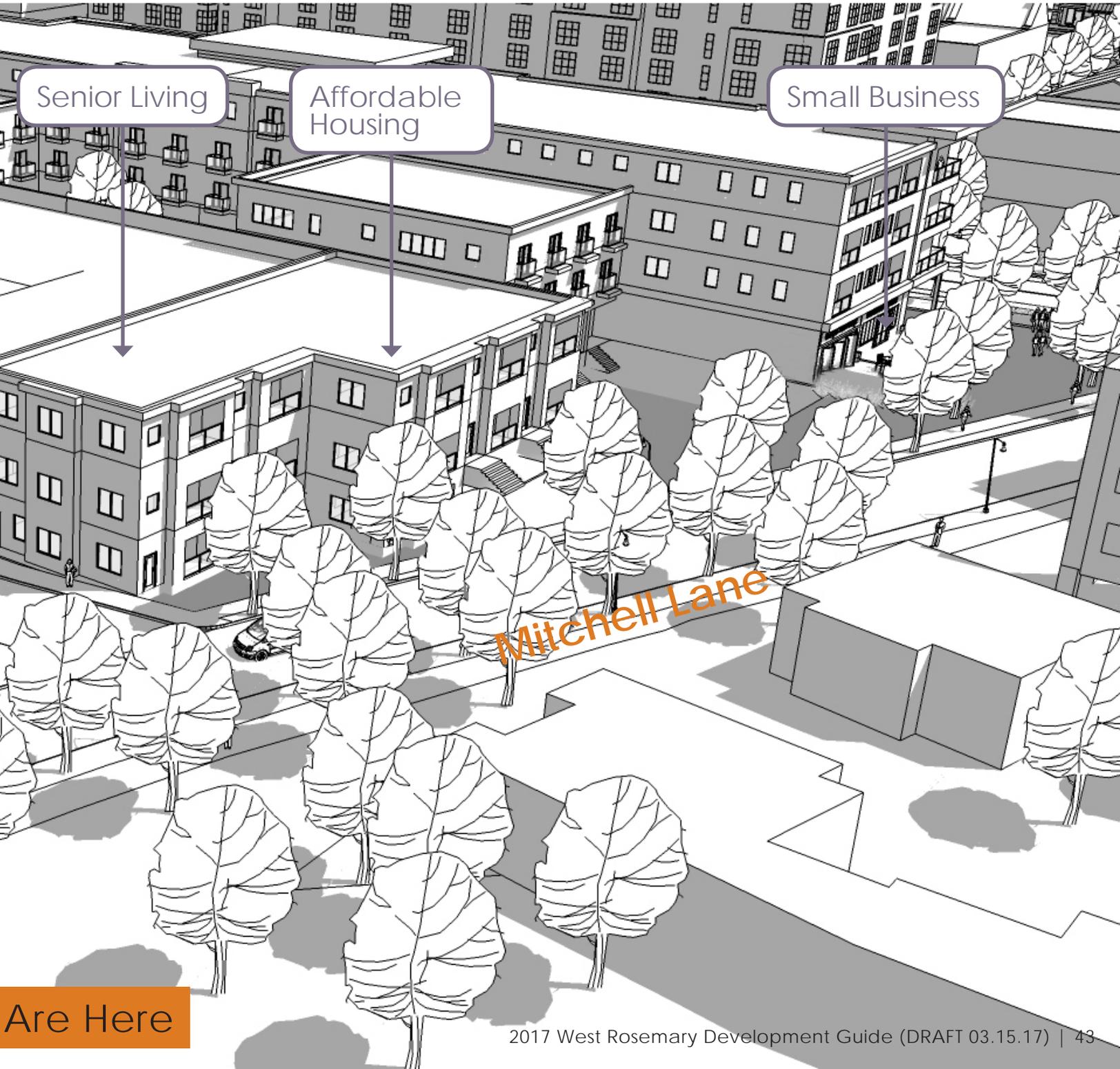
Homeownership Opportunities

Public Parking



The following image is meant to be an example of the case study site showing how community benefits could be met and where they could occur. For instance:

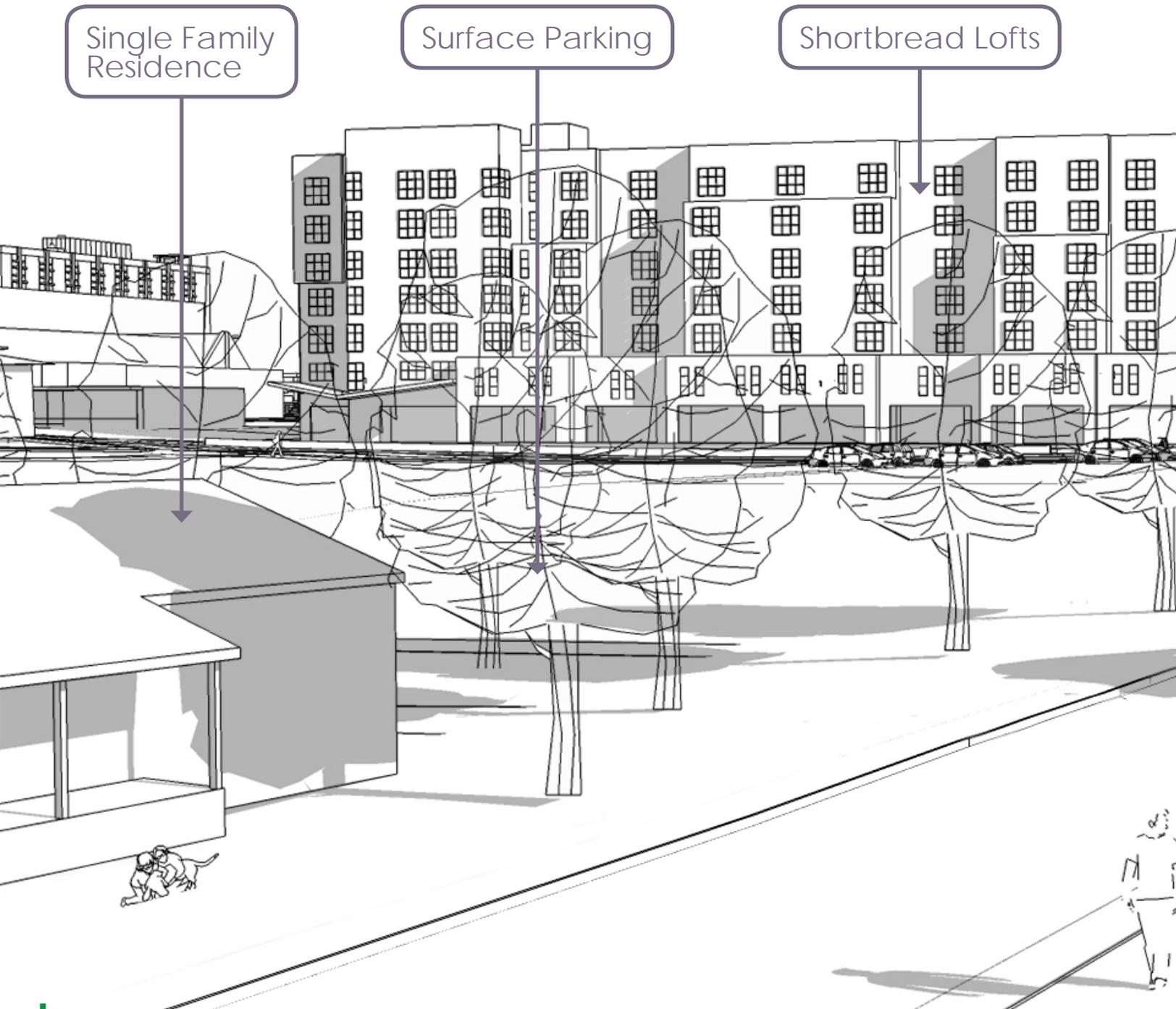
- Homeownership - opportunities would be available in the row of townhomes which form the buffer and transition to the neighborhood.
- Public parking - could be solved through a lease agreement with the developer for a certain number of public parking spaces.
- Senior living - could be included in the development mix and would be well suited as a transition from denser development to the neighborhood.
- Affordable housing - would be included through inclusionary zoning, but could be amplified through incentives.
- Small business - accommodations could be made available in traditional retail / office space or through shared retail / office space.



Are Here

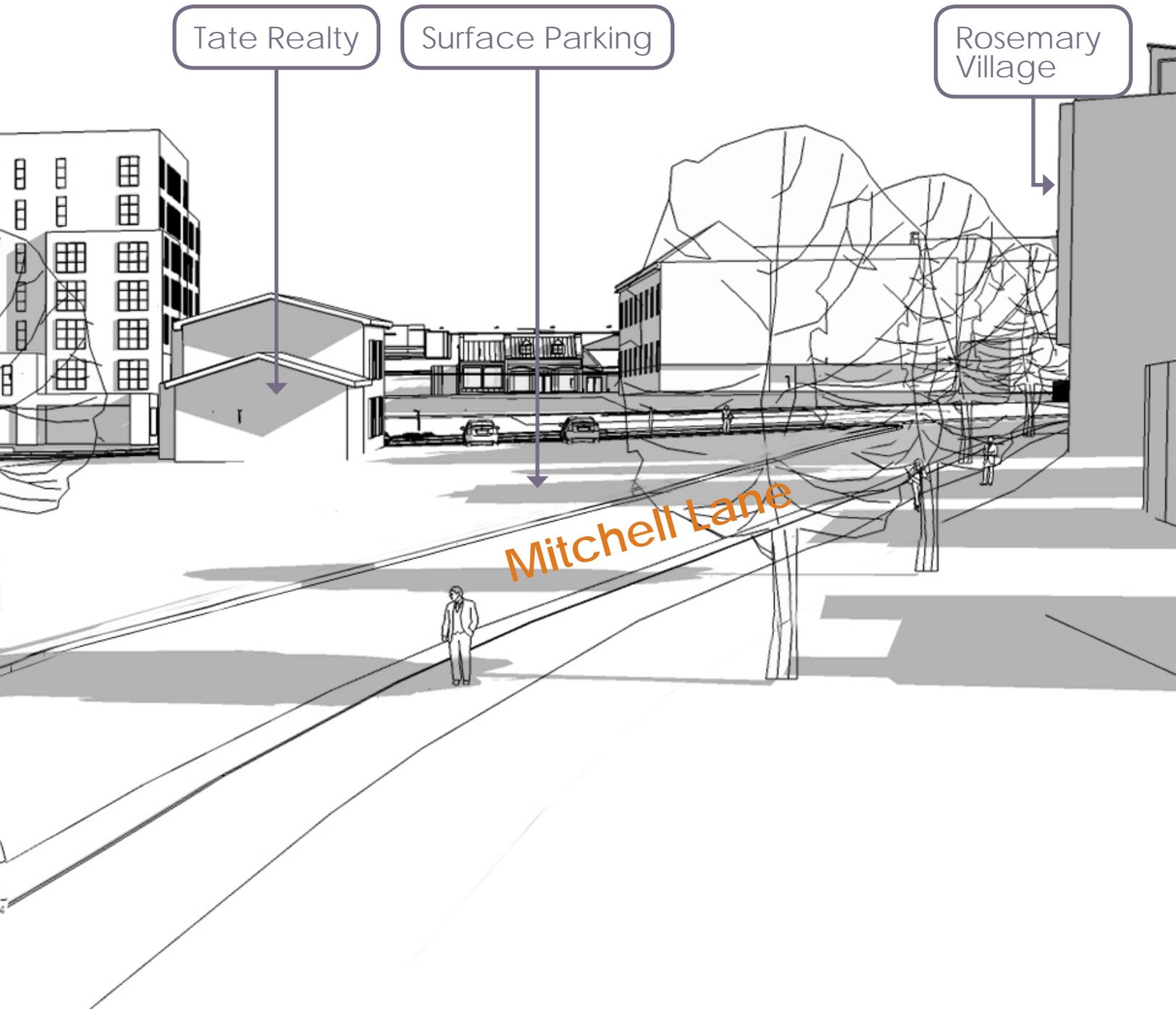
# Community Benefits

## Neighborhood Perspective





The following image shows a view from the neighborhood looking back along Mitchell Lane. The view looks across the Tate Realty parking lots towards the Shortbread Lofts, with Rosemary Village appearing just to the right. It shows the vast expanse of surface parking that makes up the transition from West Rosemary to the Northside neighborhood, and shows how uninviting the space is because of that.



# Community Benefits

## Neighborhood Perspective

Single Family Residence

Screened Parking Deck

Three-Story Residential

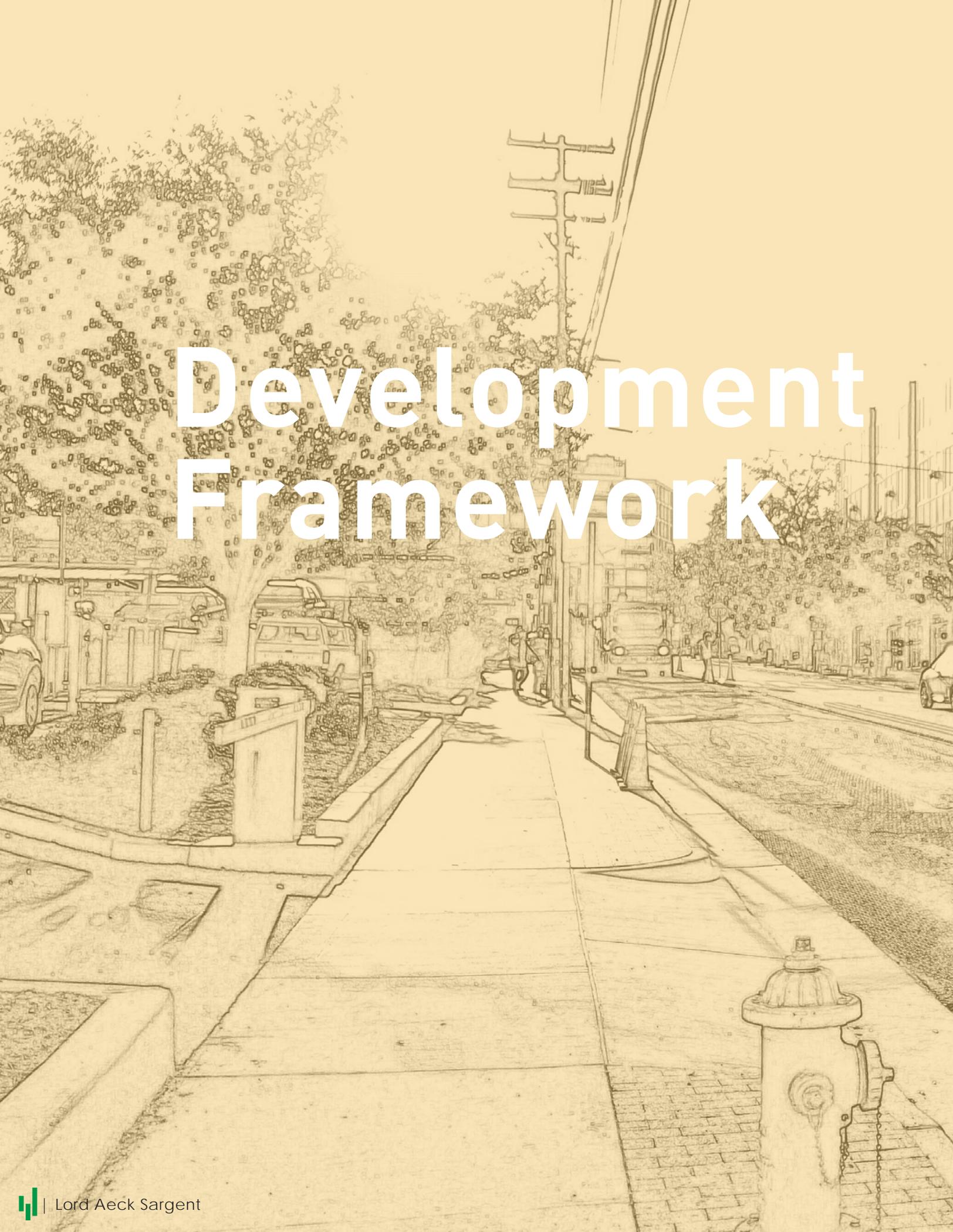
Street



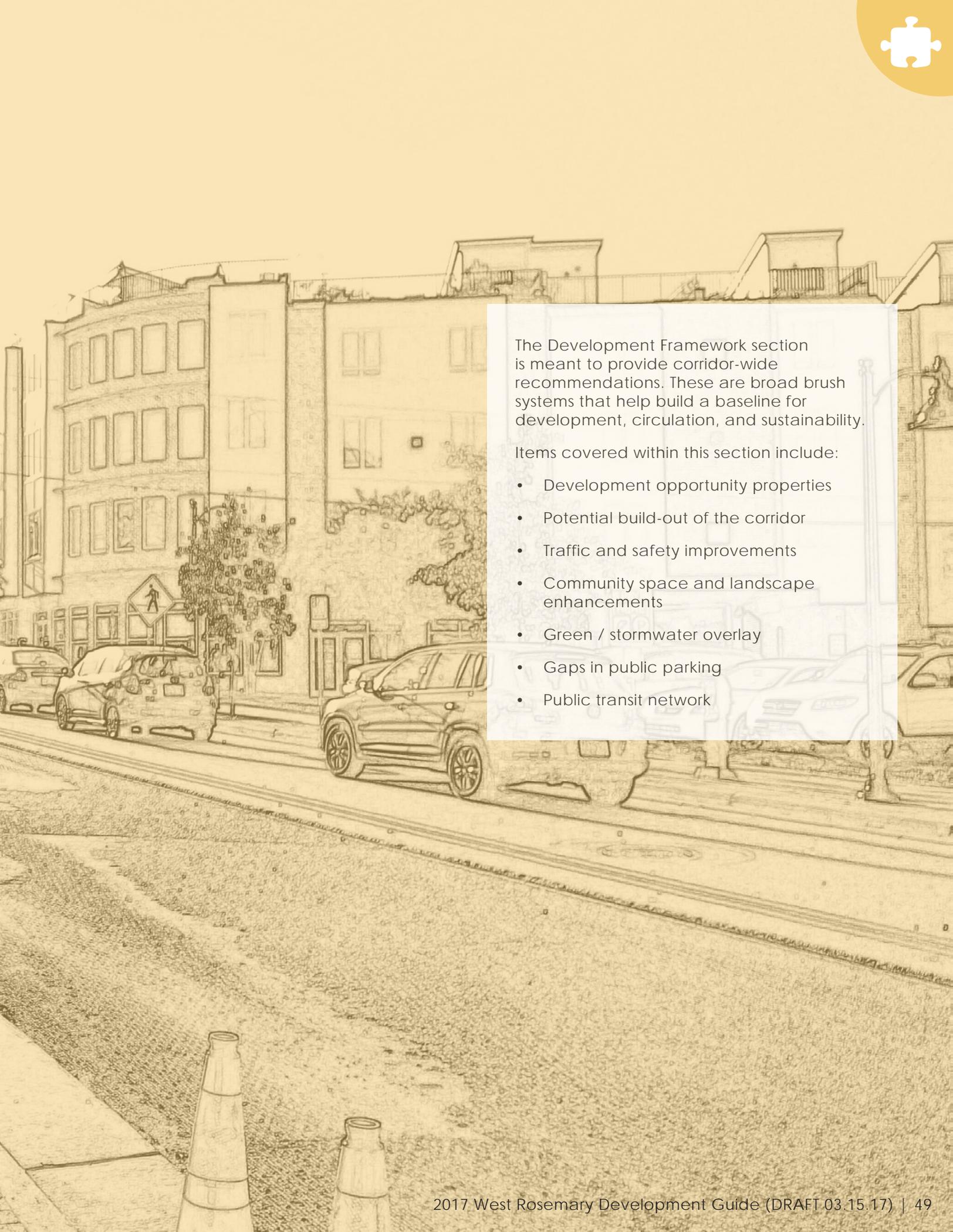


The following image shows the potential of this area to be activated through new development while still transitioning to a more humane scale approaching the neighborhood. This view looks at the rear of the development, with the liner building of multi-family residential backing onto a screened parking garage. In addition, the public realm has been improved through streetscaping and the addition of a new pocket park at the corner of Mitchell Lane and West Rosemary.





# Development Framework

The background of the page is a detailed architectural sketch of a city street scene. It shows multi-story buildings with many windows, parked cars along the curb, a pedestrian crossing sign, trees, and traffic cones in the foreground. The style is a fine-line drawing with cross-hatching for shading.

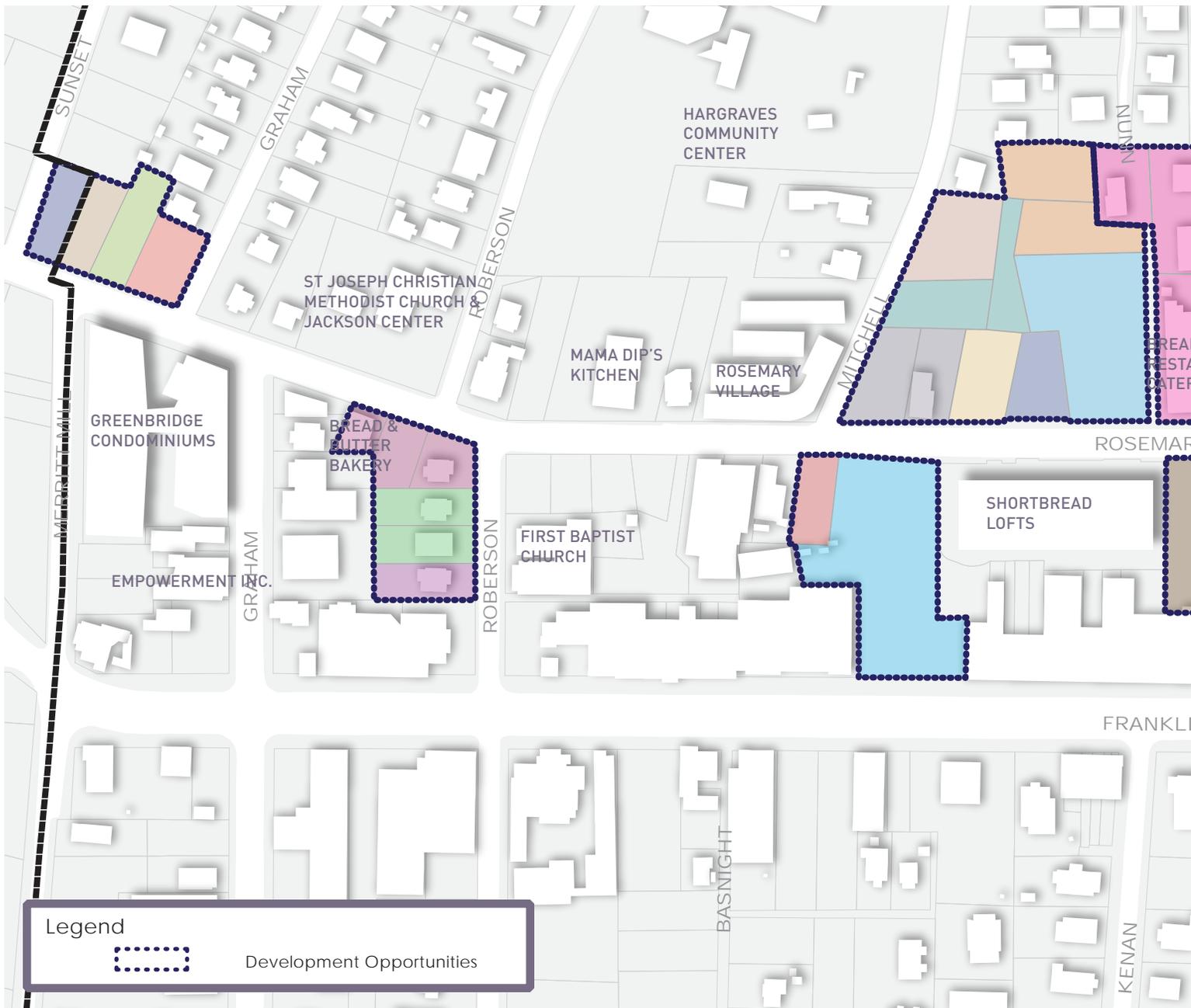
The Development Framework section is meant to provide corridor-wide recommendations. These are broad brush systems that help build a baseline for development, circulation, and sustainability.

Items covered within this section include:

- Development opportunity properties
- Potential build-out of the corridor
- Traffic and safety improvements
- Community space and landscape enhancements
- Green / stormwater overlay
- Gaps in public parking
- Public transit network

# Development Framework

## Opportunity Property by Distinct Owner



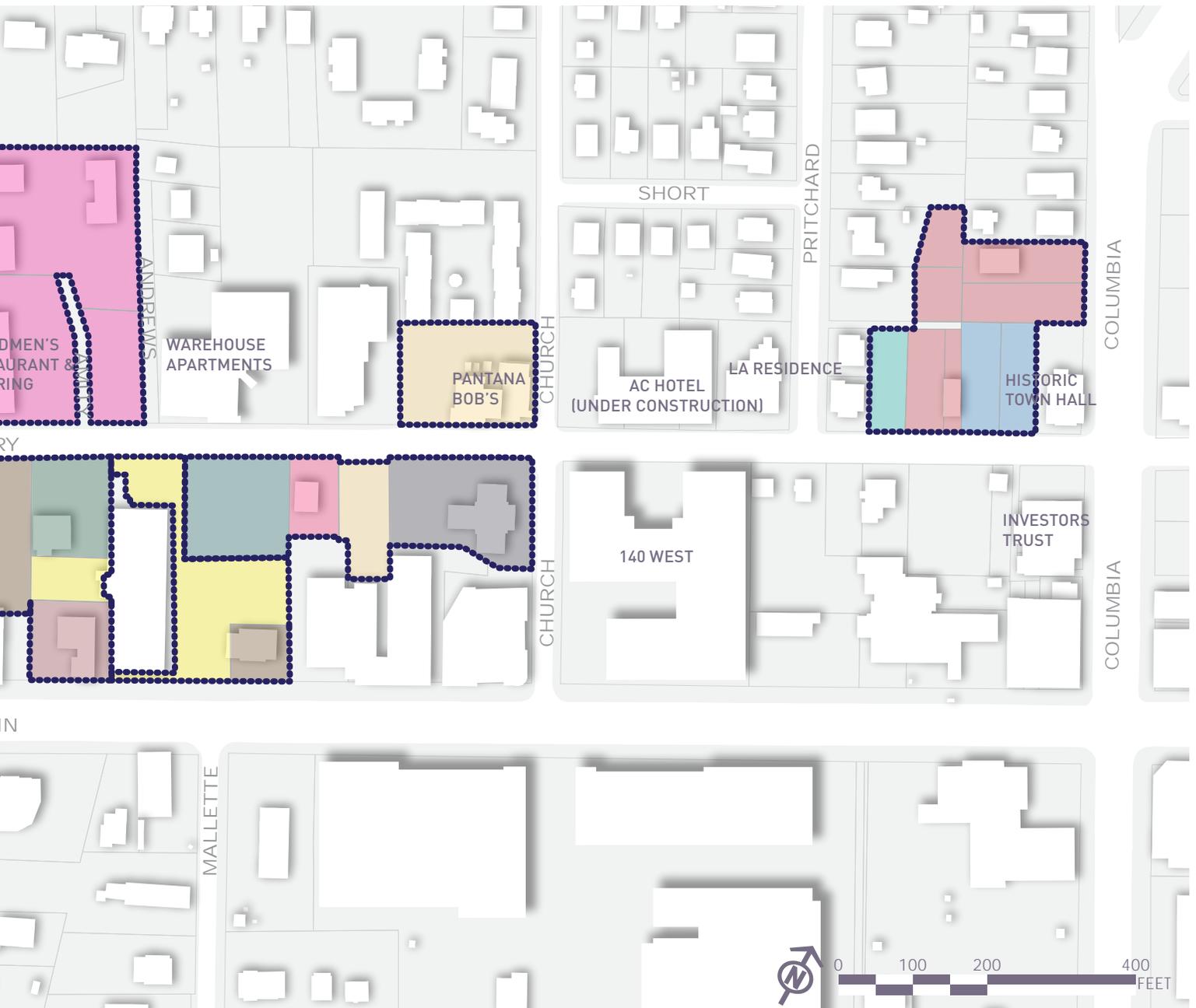


The following map represents a reasonable assessment of the realm of potential development opportunities. The colors represent different parcel owners. An example are the two parcels to the west of the Historic Town Hall that are both owned by the Town of Chapel Hill. These have been grouped into potential assemblages from the perspective of a potential developer.

**NOTE: These only represent the realm of what "MAY" likely someday redevelop and should not be considered current assemblages or the full universe of potential development sites.**

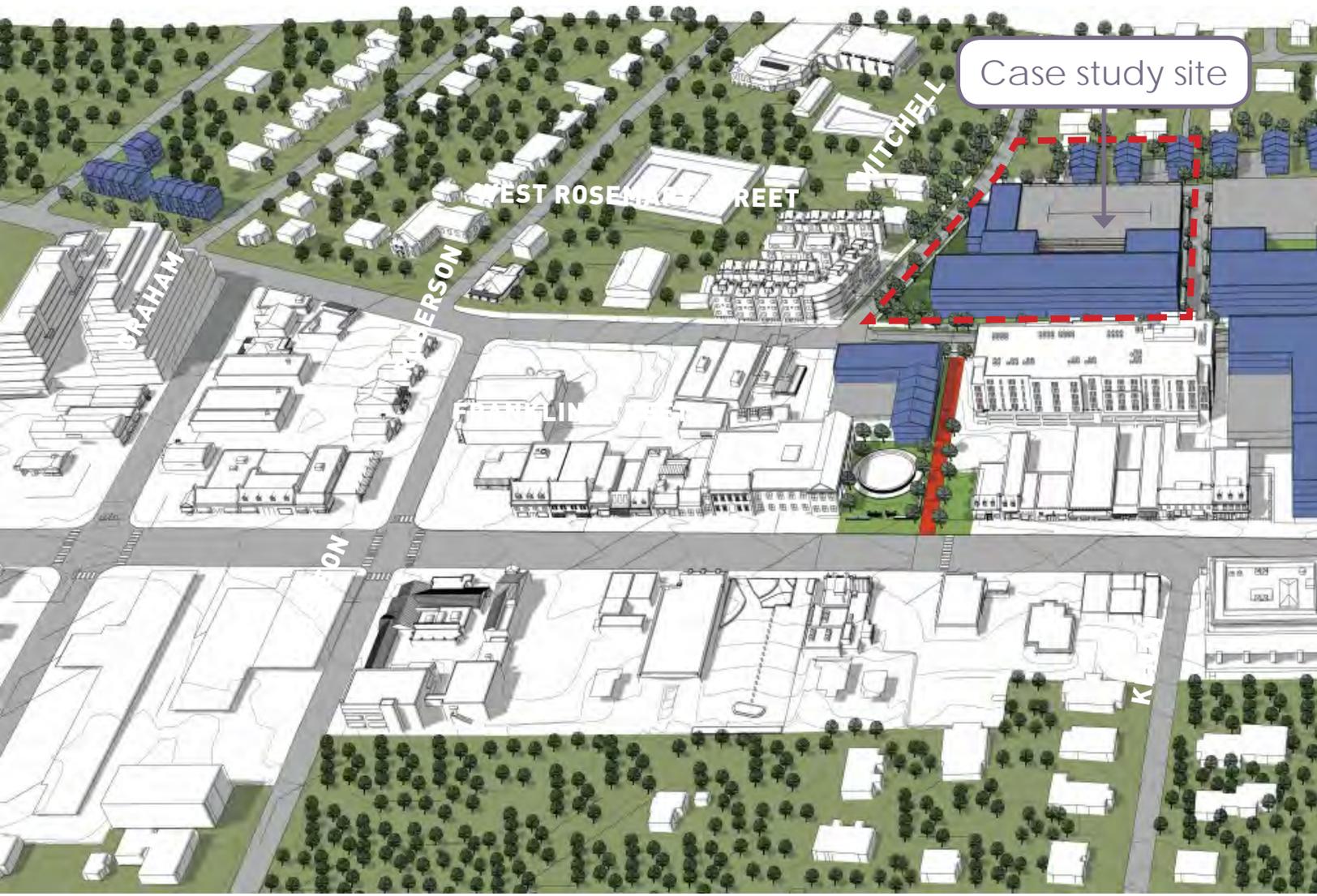
In general, a parcel was considered a potential development opportunity if they met one or more of the following (with a few exceptions):

- A vacant lot
- Majority of the site was a surface parking lot
- Existing buildings are not the highest and best use as outlined in the market analysis
- Sites adjacent had seen recent redevelopment pressure



# Development Framework

## Potential Build-Out





The massing model below shows just one potential of how the West Rosemary could potentially be built out. It is based on the potential development opportunities and assemblages identified on the previous map. The model shows the general massing of the buildings with only the floors as detail. These massings were generally tested for standard measurements and square footages, and to ensure that each site had an appropriate number of parking spaces for its assumed uses. The only articulation and details such as windows, doors, and facade treatments are shown on the building labeled as the case study. The case study was used to test the development guidelines (such as public realm and building design standards) outlined in the remainder of this document.

Several key rules of thumb were used to guide the design of the model.

- Line both sides of the street with infill development to activate the street
- Limit the height on the north side of the corridor to be the minimum assumed by market demand
- Transition building heights down to adjacent residential and buffer neighborhoods

- Allow increased density on the south side of the street
- Limit the amount of surface parking and use structured parking where possible
- Provide space for pedestrian connections and pocket parks

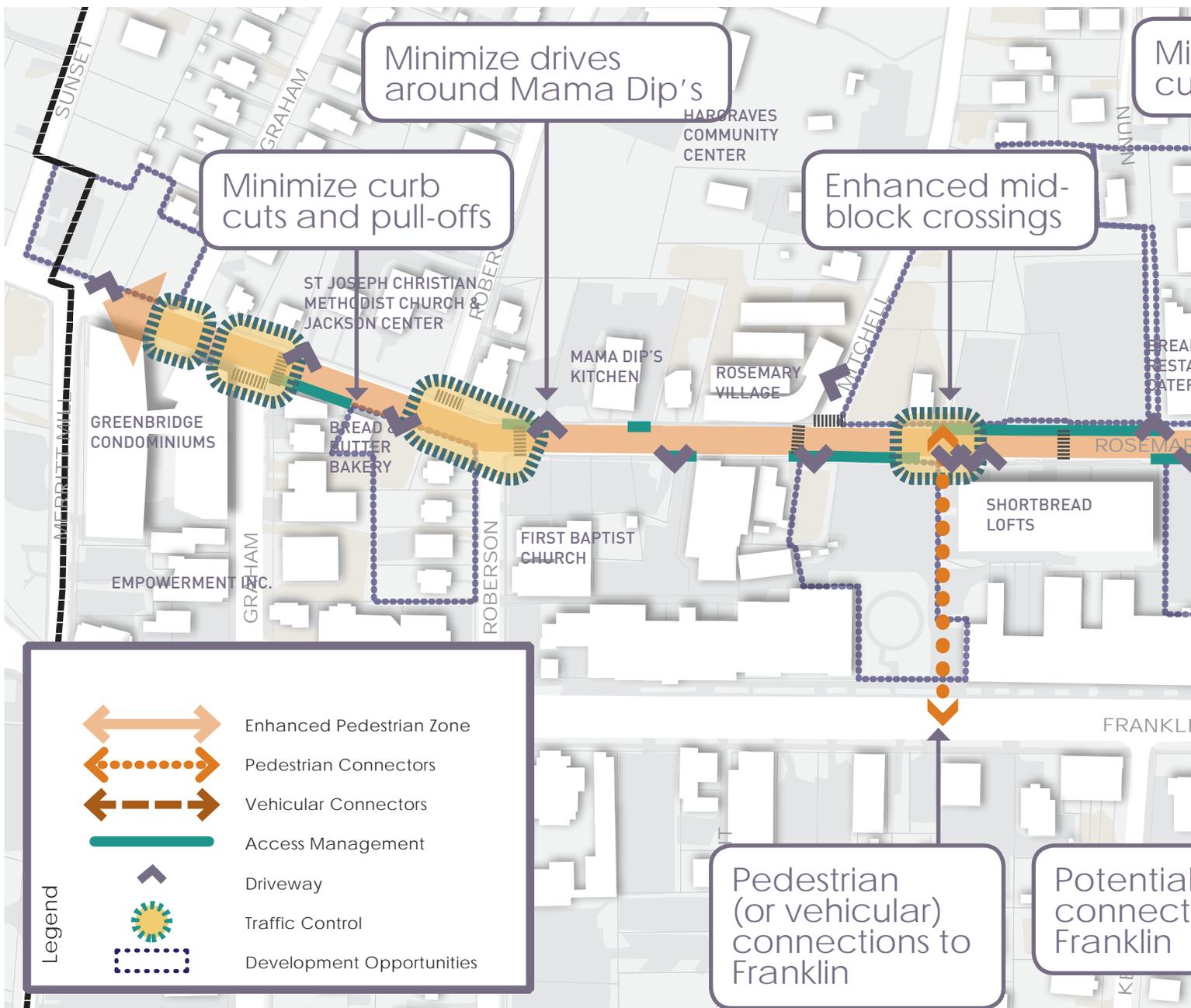
Based on the case study model and the market analysis, the Town may want to consider a revision of the NCD to allow a maximum allowable building height of four floors, or 48 feet (assumed 15 feet for the ground floor and 11 feet for the remaining floors) on the north side of West Rosemary. The current standards only allow for 40' as the primary building height, which doesn't actually allow for a typical four stories in new construction.

**NOTE: These only represent the realm of what "MAY" likely someday redevelop and should not be considered current assemblages or the full universe of potential development sites.**



# Development Framework

## Traffic and Safety Improvements



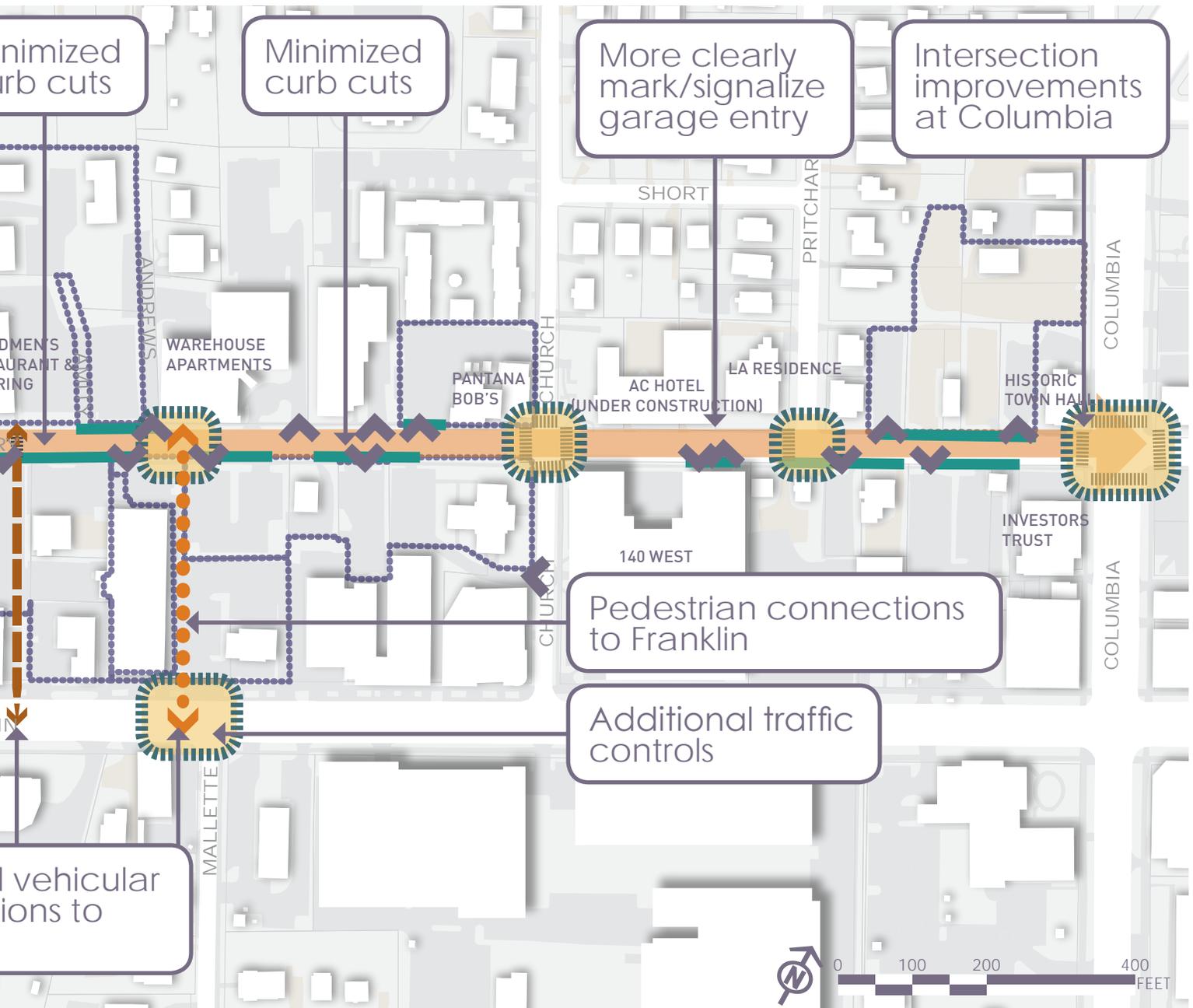


In addition to recent streetscape improvements, additional measures should be considered to enhance safety and improve traffic. One of the most easily attainable objectives will be to minimize the number of vehicular access points and drives. This will minimize the potential for pedestrian/vehicular conflicts, as well as return more room for streetscape improvements.

Raised crosswalks and intersections will elevate pedestrians so that they are more visible to vehicles and will give greater visual presence to pedestrian zones along the street. In addition, the raised grade will force cars to slow down as they traverse intersections, thereby improving the safety of the roads and mitigating conflicts between cars and pedestrians. - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009.

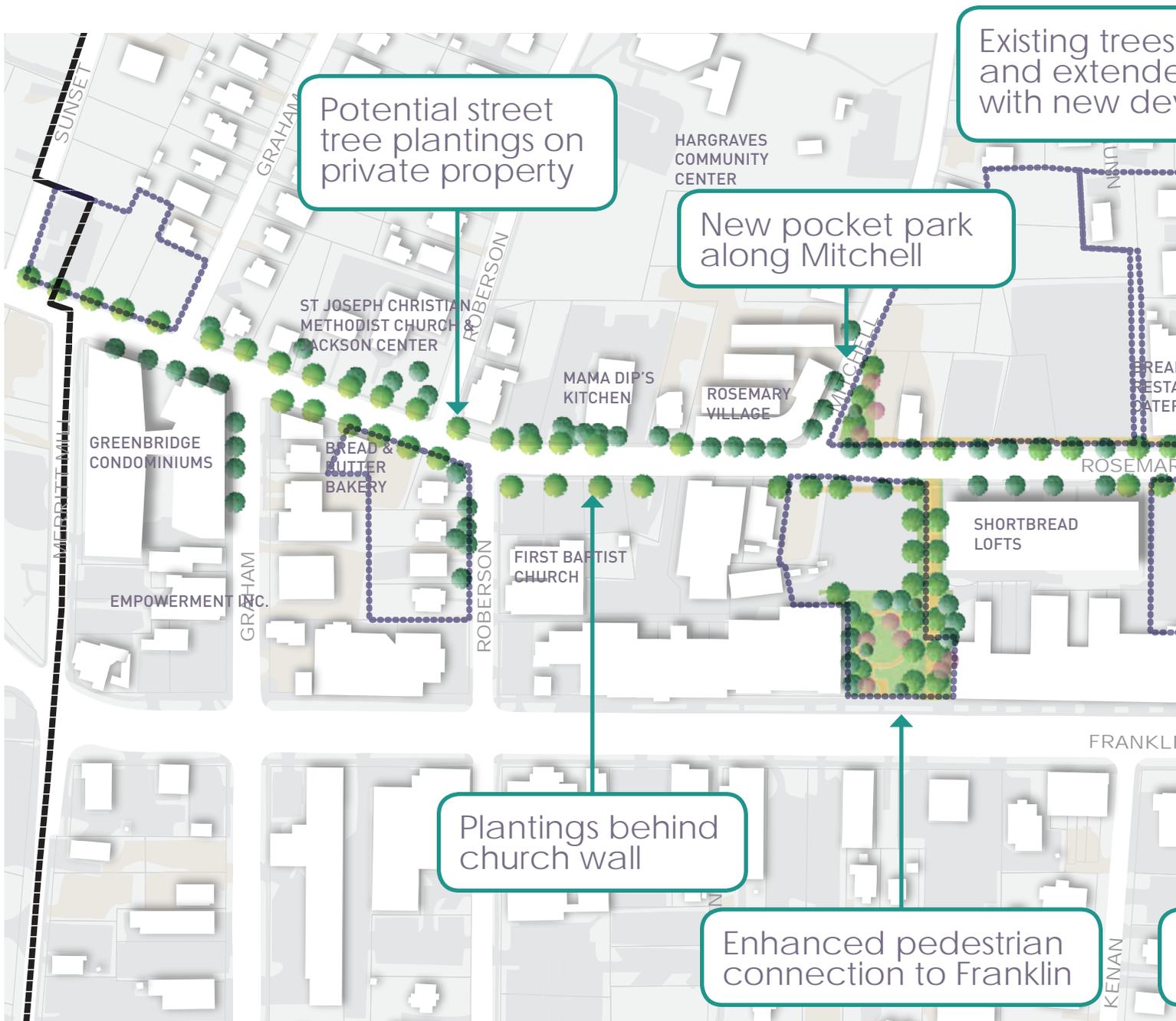


**NOTE:** Issues of egress and fire safety must be taken into account when considering improvements.



# Development Framework

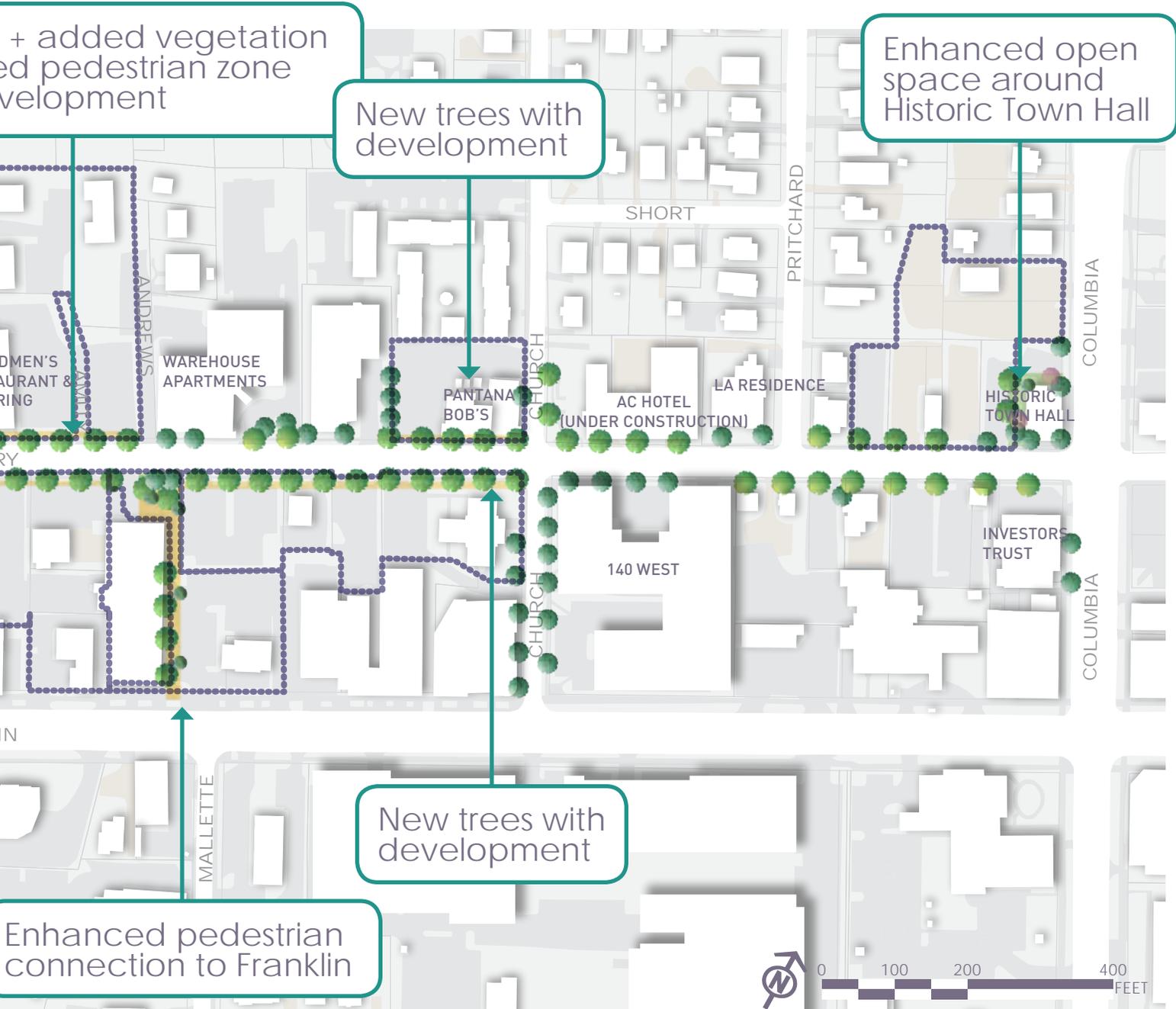
## Community Space/Landscape Enhancements





Continue the improvements outlined in the Chapel Hill Streetscape and Lighting Master Plan, including additional street trees with new development, acquisition of right-of-way or easements on private property for additional street trees, and the burying of utility lines for a more generous overhead canopy.

The plan also outlines inclusion of pocket parks, or “nano parks” as they were called in the Chapel Hill Streetscape and Lighting Master Plan, can act as small areas of public open space with direct access from the street. These parks could include art and signage that could resonate with the cultural identity of the Northside Neighborhood.



+ added vegetation  
pedestrian zone  
development

New trees with  
development

Enhanced open  
space around  
Historic Town Hall

New trees with  
development

Enhanced pedestrian  
connection to Franklin

# Development Framework

## Community Space/Landscape Enhancements





This image shows the pedestrian alley condition, looking from West Rosemary towards Franklin Street, adjacent to the Launch Building as it occurs today.

# Development Framework

## Community Space/Landscape Enhancements





This image shows the potential of this pedestrian connection to become a gathering space for the community and an asset to adjacent retail space.

# Development Framework

## Green / Stormwater Overlay

### 1 rain gardens within street improvements

Rain gardens can be placed within proposed streetscape and public realm improvements to capture stormwater runoff sheeting off the road.

The plants and soil of these rain gardens will help to filter storm water runoff from roadways and sidewalks. Rain garden plantings are composed of perennial plants that can contribute to the surface water mitigation and will be hardy in soil conditions that reach saturated and dry conditions. - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009.

This may seem contrary to the most recent streetscape improvements which included additional hardscaping, but it can be done in targeted stretches initially focused on areas of new development, and is in all reality a preferable solution from a sustainability standpoint.

### 2 stormwater features within pocket parks

Riparian water features (which can be dry beds when there is no rain) can be placed within the proposed open spaces and pocket parks to help collect and treat stormwater. These features can also have an educational component to them

### 3 green roofs on new development

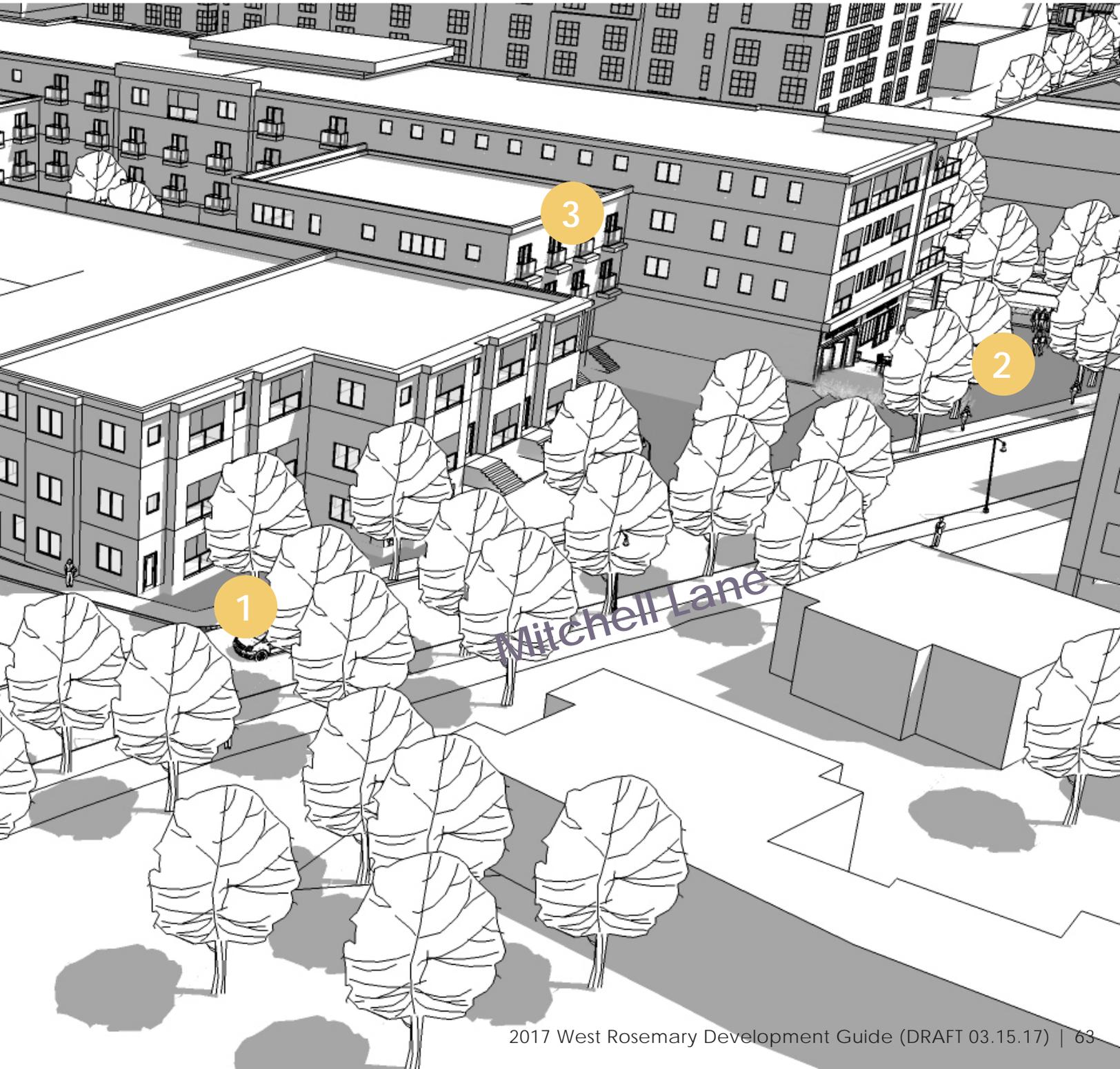
A portion of roofs on new developments could be constructed into green roofs to help collect rain water.

### 4 rainwater collection in underground cisterns

Underground cisterns could potentially be used to capture rainwater off the non-vegetative roofs. This water could then be used for irrigation, rather than using municipal water.

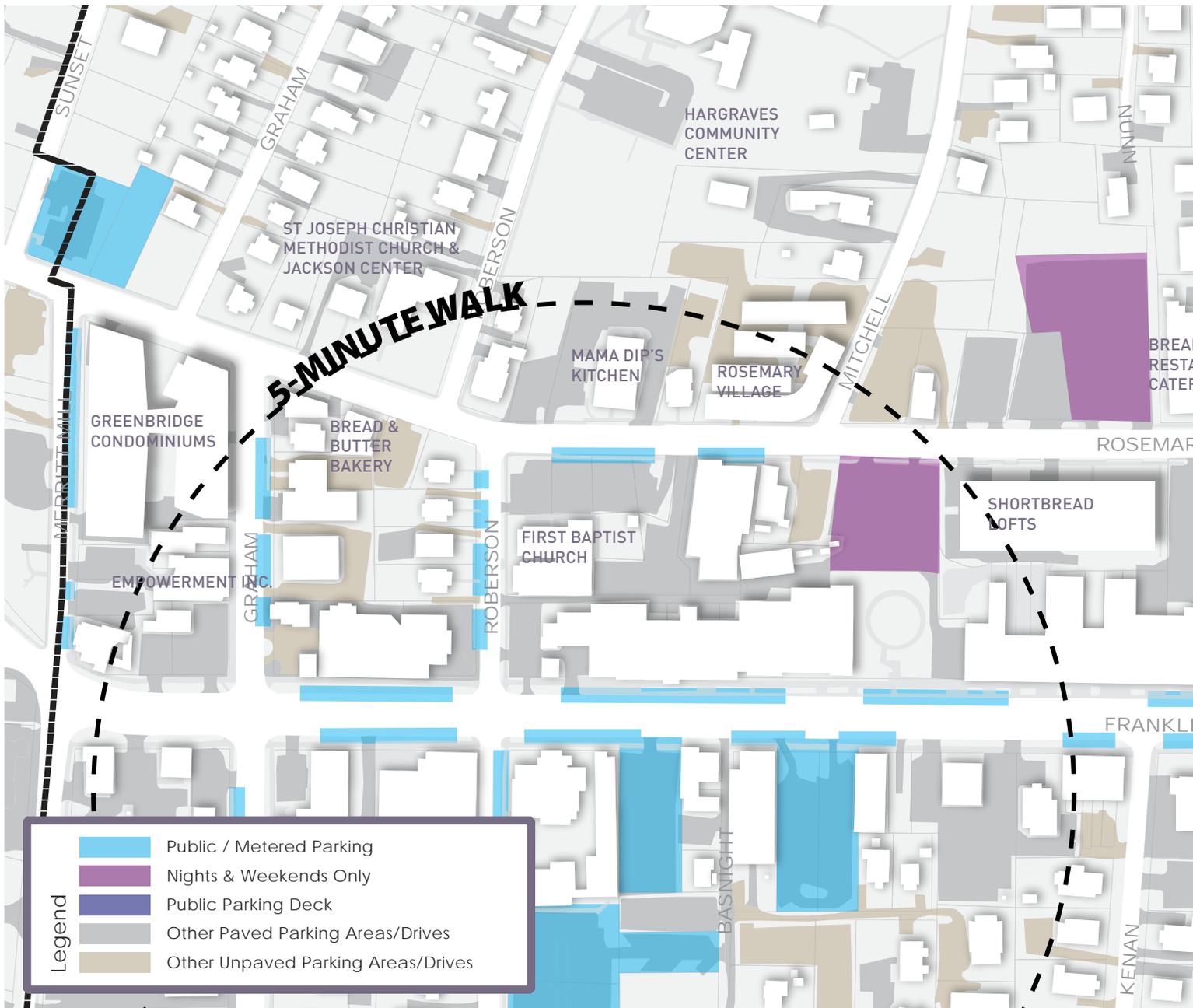


Given the Town's standards that the stormwater runoff rate leaving the site post-development shall not exceed the stormwater runoff rate leaving the site pre-development it seems that there could be a number of creative solutions to capturing, collecting, and even reusing that stormwater on-site. This is especially critical as much of West Rosemary deals with constant stormwater issues, sitting in a low point of the watershed.



# Development Framework

## Gaps in Public Parking





Based on feedback from the public there are concern about:

- Amount of parking - between the residents, students, and patrons of local businesses there don't appear to be enough parking spaces.
- Hours served - many of the lots along Rosemary are available to the public only in the evening.
- Proximity to retail, business, and other points of interest - where parking does exist it is not near the places people want to frequent.

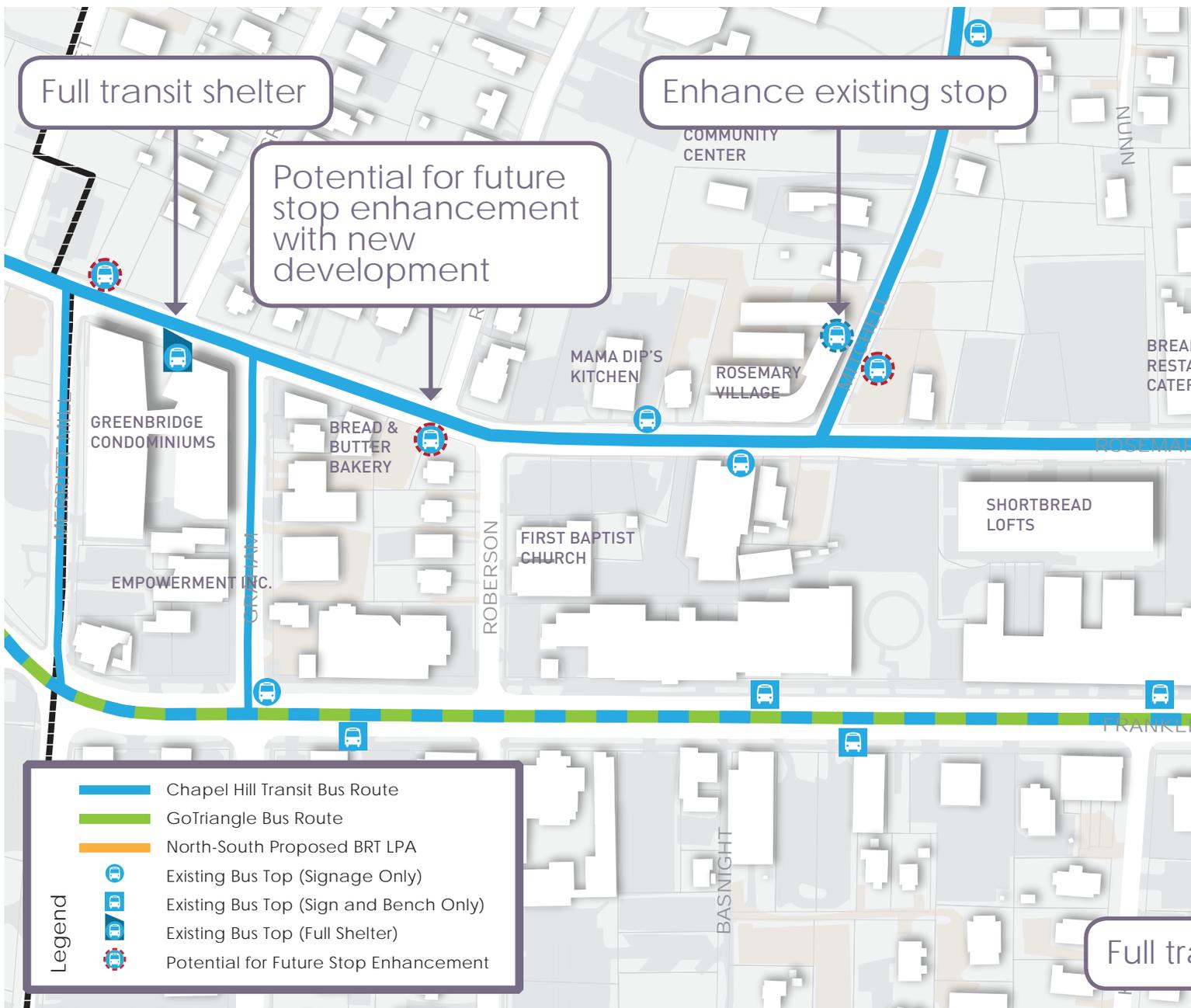
There are areas today that are underserved during regular business hours, This is represented through the 5-minute walk circles, what is considered a comfortable walk, around either the surface and metered parking to the west, or the structured deck at 140 West. Additional more centralized parking could achieve the desire for a park-once mentality shift as heard by the public.

Future parking could better be served through clearer wayfinding throughout Downtown which would guide people to public parking locations.



# Development Framework

## Public Transit Network



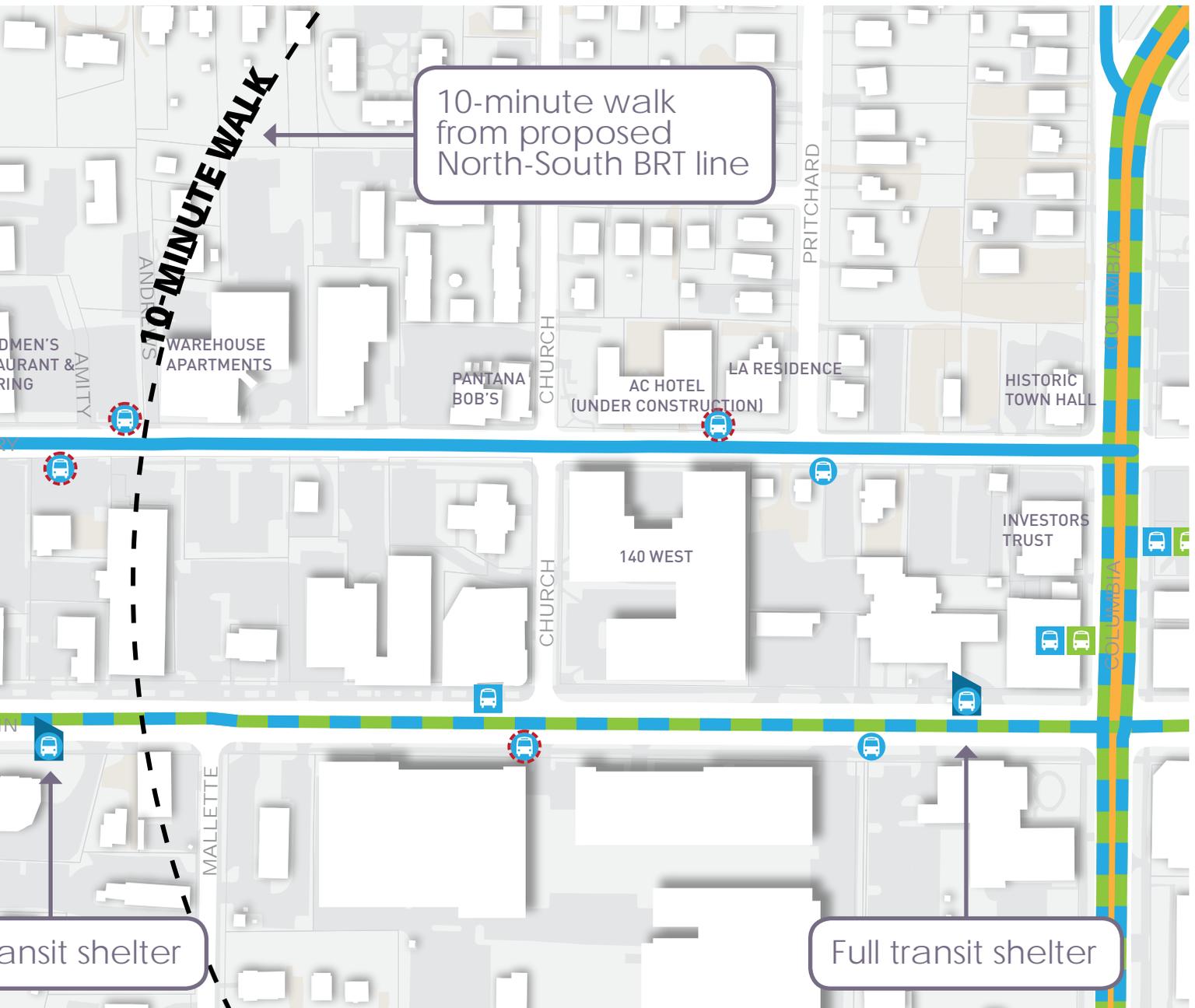


The area around West Rosemary is well-served by GoTriangle and Chapel Hill buses today, with stops along West Rosemary (Route CW) or Franklin Street (Routes F and J), and even running into the Northside Neighborhood (Route A) along Mitchell Lane, not to mention the countless routes that circulate up and down Columbia and through campus. The buses generally have 30-minute headways, evening, and condensed weekend service. The maximum a person will typically walk to get to a bus stop is 5-minutes (a quarter mile). Based on that the entirety of the West Rosemary corridor is served by bus transit.

Although the use of real time bus info on cell phones helps to cut down on the average wait time at a stop, there remains the issue for those that may not have access to that technology being left waiting for extended periods in the elements at a stop.

The future construction of the North-South BRT corridor along Columbia will increase service and mode options in the area. A person is typically willing to walk up to 10 minutes (a half mile) to a bus rapid transit (BRT) line, which means that at least half of the corridor is within a comfortable walk.

There was feedback that the existing bus stops were generally not clearly marked with large enough signage and schedules, and they lacked any sort of shelter or even a bench for waiting.





# Public Realm



The Public Realm section covers recommendations that could be applied to all of the areas between buildings. Generally, these are the spaces that the public occupies along streets or within plazas, and provides a place with a human-scale feel.

The notion is that these guidelines can be encouraged within new development, but existing streetscape can eventually be retrofitted to meet these as well.

Items covered within this section include:

- Enhancement of the pedestrian experience
- Making public spaces more inviting
- Easing pedestrian circulation
- Hiding unsightly but necessary features
- What a typical street looks like today and what it could look like
- How this could be applied over time along West Rosemary Street

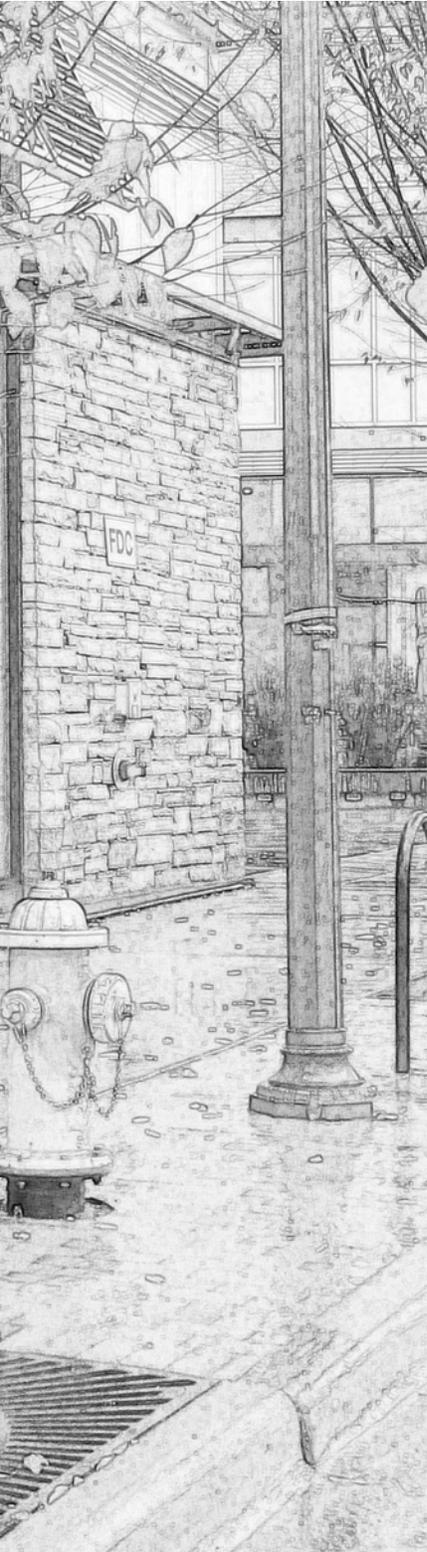
# Public Realm

Enhance the pedestrian experience





The following guidelines were created in accordance with the 2009 Chapel Hill Downtown Streetscape and Lighting Master Plan. Generous tree-lined sidewalks encourage pedestrian activity at the ground level, help the feasibility of retail space, and provide a buffer between vehicles and pedestrians. Requiring a minimum sidewalk width on any new development projects ensures that the pedestrian zone feels comfortable for two or more people to walk next to each other, and for others to pass alongside.



## 1 provide sufficient space for pedestrians

The clear pedestrian zone **MUST** be at least 10 feet wide along major retail or mixed-use blocks (such as Rosemary Street) and at least 5 feet wide on all other blocks. – LEED ND, 2014.

## 2 provide continuous sidewalks

Continuous sidewalks **MUST** be provided along 90% of any new circulation block length. Alleys may be exempted. – LEED ND, 2014.

## 3 require street tree plantings

Provide street trees along at least 60% of the total planned block length, between the vehicle travel way and walkway. – LEED ND, 2014.

Downtown Chapel Hill's trees should be planted primarily as a family, not as individual specimens. This will produce the qualities of a continuous canopy, high branched trunks, and filtered light. To achieve this effect, trees of similar but not identical form should be planted at varied distances from one another, typically 20 to 30 feet apart (excluding driveways and intersections). Each tree should have no branches lower than 7 feet in height at the time of planting. Further "limbing up" to 10 to 16 feet as part of a regular pruning program will encourage the formation of a high continuous canopy. - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009.

\* *LEED Reference Guide For Neighborhood Development*. 4th ed. Washington, D.C.: N.p., 2014. Print.

# Public Realm

Make public space more inviting

## 4 provide sufficient amenities

The Master Plan encourages placement of benches only within public open spaces and at bus stops and suggests limiting future installation of benches along sidewalks unless specifically supported by businesses. Benches should remain in bus stop areas and at corners where curb extensions have been installed that create wider gathering spaces. Given that 75% of blocks along Rosemary and Franklin Streets have two or more bus stops, ample seating will be distributed throughout the Downtown District. - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009.

## 5 add welcoming & safe lighting

In addition to improving the aesthetics of the streetscape, lighting fulfills the important function of illuminating roads and sidewalks, reducing the probability of traffic-related accidents and improving public safety. Require that any additional lighting comply with the Illuminating Engineering Society of North America (IESNA) standards.



5

4

# Public Realm

Ease pedestrian circulation





## 6 clearly mark paths & wayfinding

Increase the number of path markings and wayfinding/informational signage to points of interest and key pedestrian connections. This signage and wayfinding can also be tied to the cultural identity of the neighborhood.

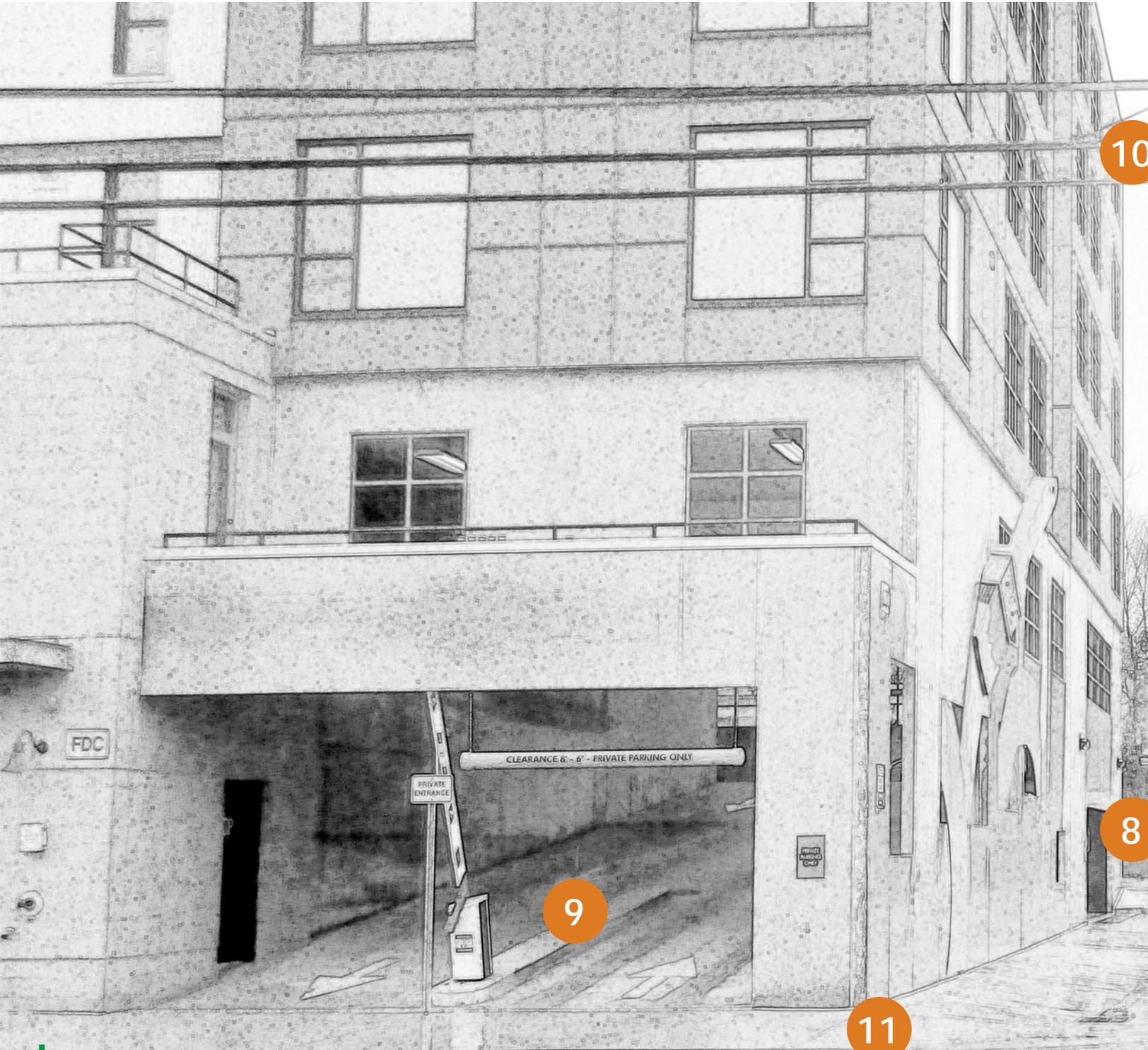
## 7 provide enhanced pedestrian crossings

Raised crosswalks and intersections will elevate pedestrians so that they are more visible to vehicles and will give greater visual presence to pedestrian zones along the street. In addition, the raised grade will force cars to slow down as they traverse intersections, thereby improving the safety of the roads and mitigating conflicts between cars and pedestrians. - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009.



# Public Realm

Hide unsightly but necessary features



10

8

9

11



While infrastructure is a necessary part of any new development there are ways to mitigate its visual impact and effect on the public realm. Screening infrastructure, such as dumpsters and service areas, and hiding parking allows the focus to be on the building itself. Reducing the number of driveways to parking and service areas also reduces the number of potential pedestrian conflicts with vehicles.

## 8 screen infrastructure

Mitigate the visual impact of necessary infrastructure through architectural screening techniques or by placing infrastructure behind a seal-able door.

## 9 hide or screen parking

The visual impact of parking can be lessened by placing it in underground or screened structured parking, or by placing it off the main roadway, behind the building and wrapping it.

## 10 bury utility lines

Work with Duke Energy to bury the overhead utility lines that are impeding successful street tree canopies and detracting from the visual aesthetics of the street. - Chapel Hill Downtown Streetscape and Lighting Master Plan, 2009.

This would require a system-wide investment by the Town and although it is cost-prohibitive for a single project the impact would be immense. Residents and business owners echoed their support.

## 11 reduce the number & size of curb cuts

No more than 20% of the block length of the circulation network is faced directly by garage and service bay openings. - LEED ND, 2014.

\* LEED Reference Guide For Neighborhood Development. 4th ed. Washington, D.C.: N.p., 2014. Print.

# Public Realm

## Typical Street Section



Existing Streetscape

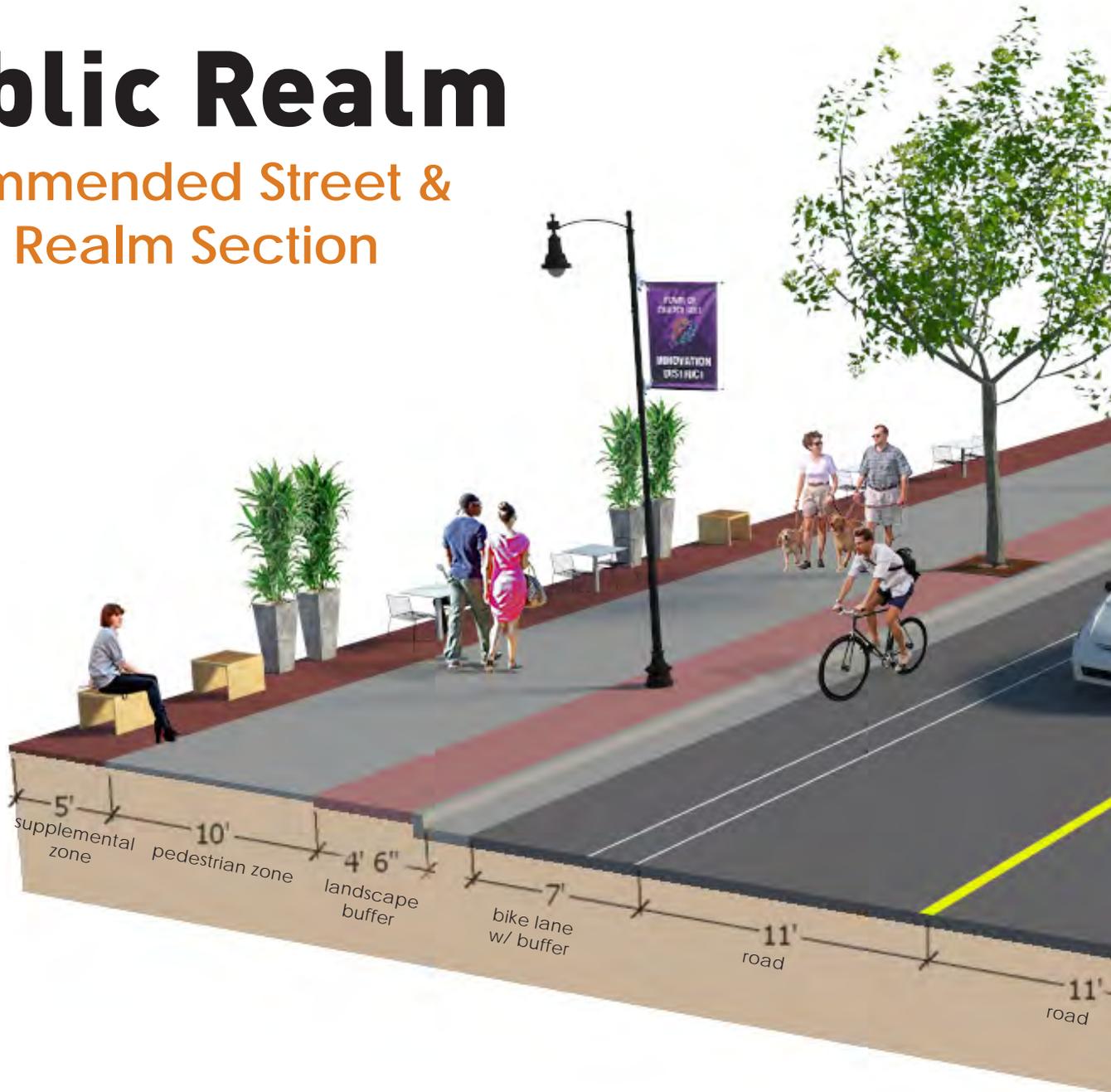


2'  
5'  
4' 6"  
5'

bike lane  
lighting +  
landscape  
pedestrian zone  
(varies)

# Public Realm

## Recommended Street & Public Realm Section

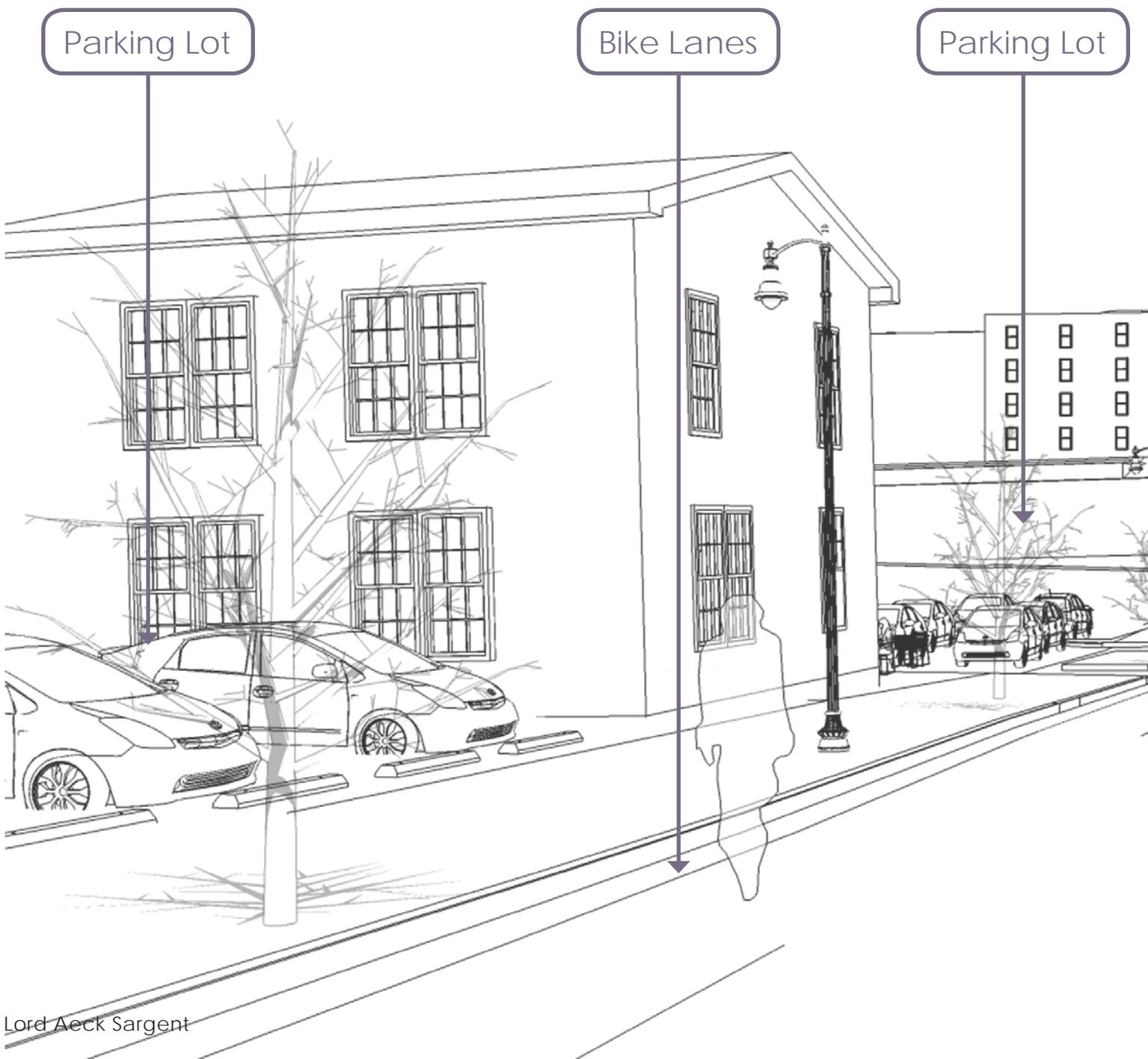


Streetscape Improvement



# Rosemary Street Perspectives

## Existing View



Parking Lot

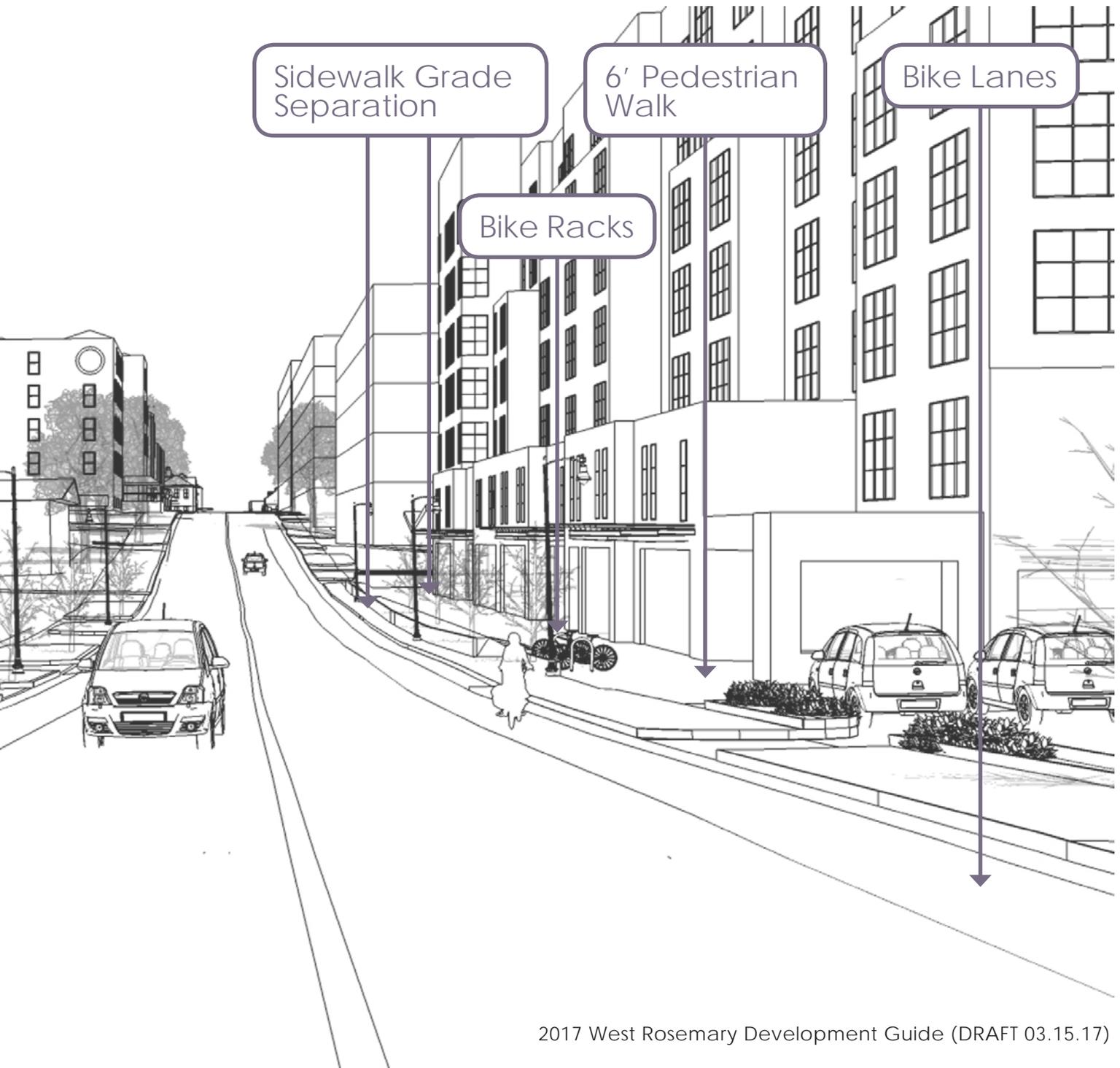
Bike Lanes

Parking Lot



The following series of images show the progression of the way the public realm could be improved along West Rosemary in the short term and then later on with potential new development.

The image below shows the existing conditions on Rosemary following the most recent streetscape improvements. The roadway is a typical two-way street with bike lanes (in some narrow points sharrow) along either side. The pedestrian walkways are exceedingly narrow for an urban context, in many cases only six feet wide. In many cases surface parking lots abut the sidewalks and create an unappealing pedestrian condition.



# Rosemary Street Perspectives

## Public Realm Improvements

Supplemental Zone

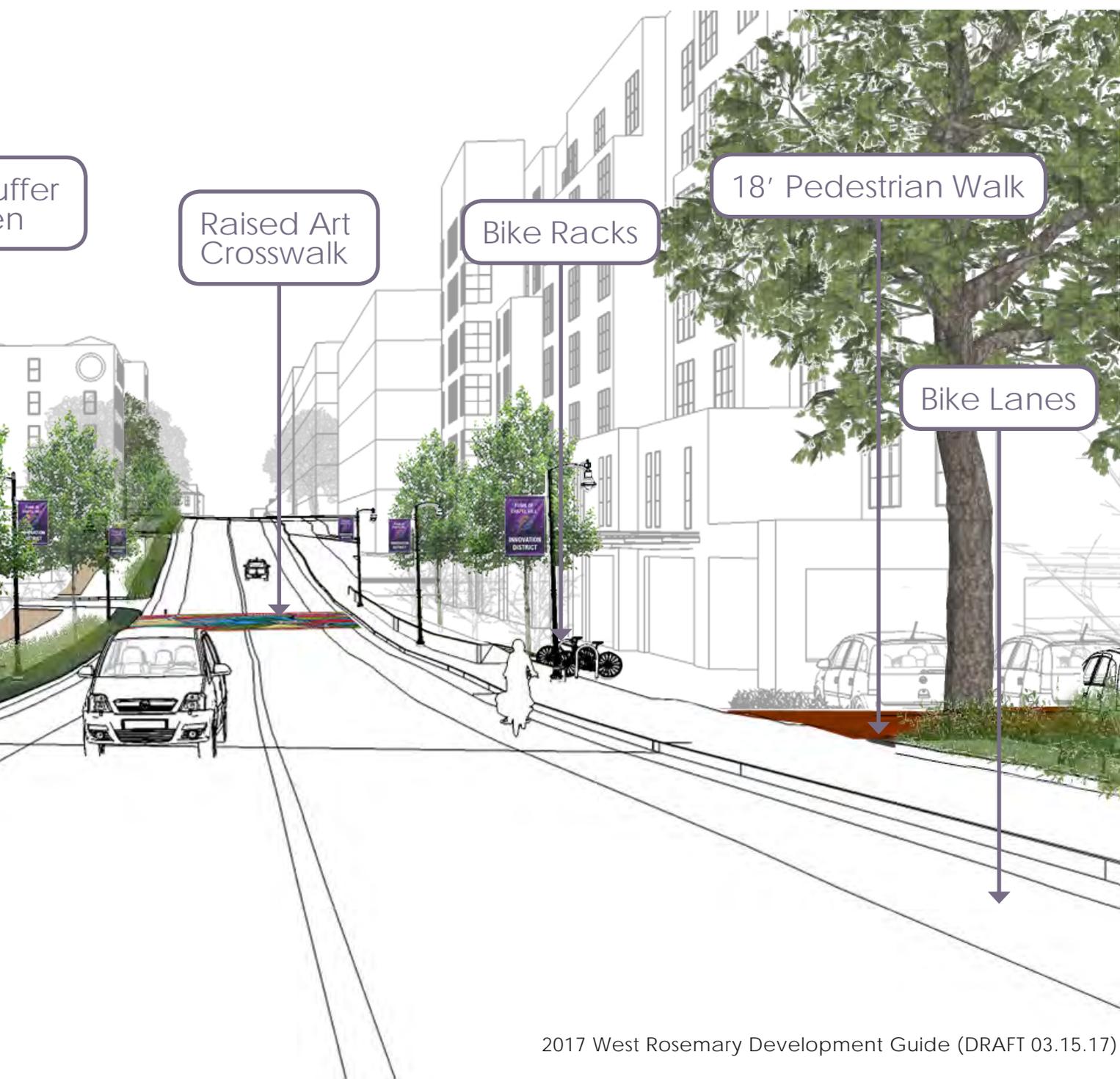
10' Sidewalk

Landscape Buffer or Rain Garden





The image below shows how the street would look and feel if the improvements outlined in this section were put in place regardless of new development. Those improvements call for more generous sidewalks and supplemental zones behind the sidewalks for street furniture, outdoor dining. Additional landscaping comes in the form of added street trees and landscaped buffers or rain gardens for stormwater management between the pedestrians and vehicles. Added signage and wayfinding provide a sense of place and orientation.



# Rosemary Street Perspectives

## Future Development Character

Supplemental Zone

Exterior Seating

Corner Park

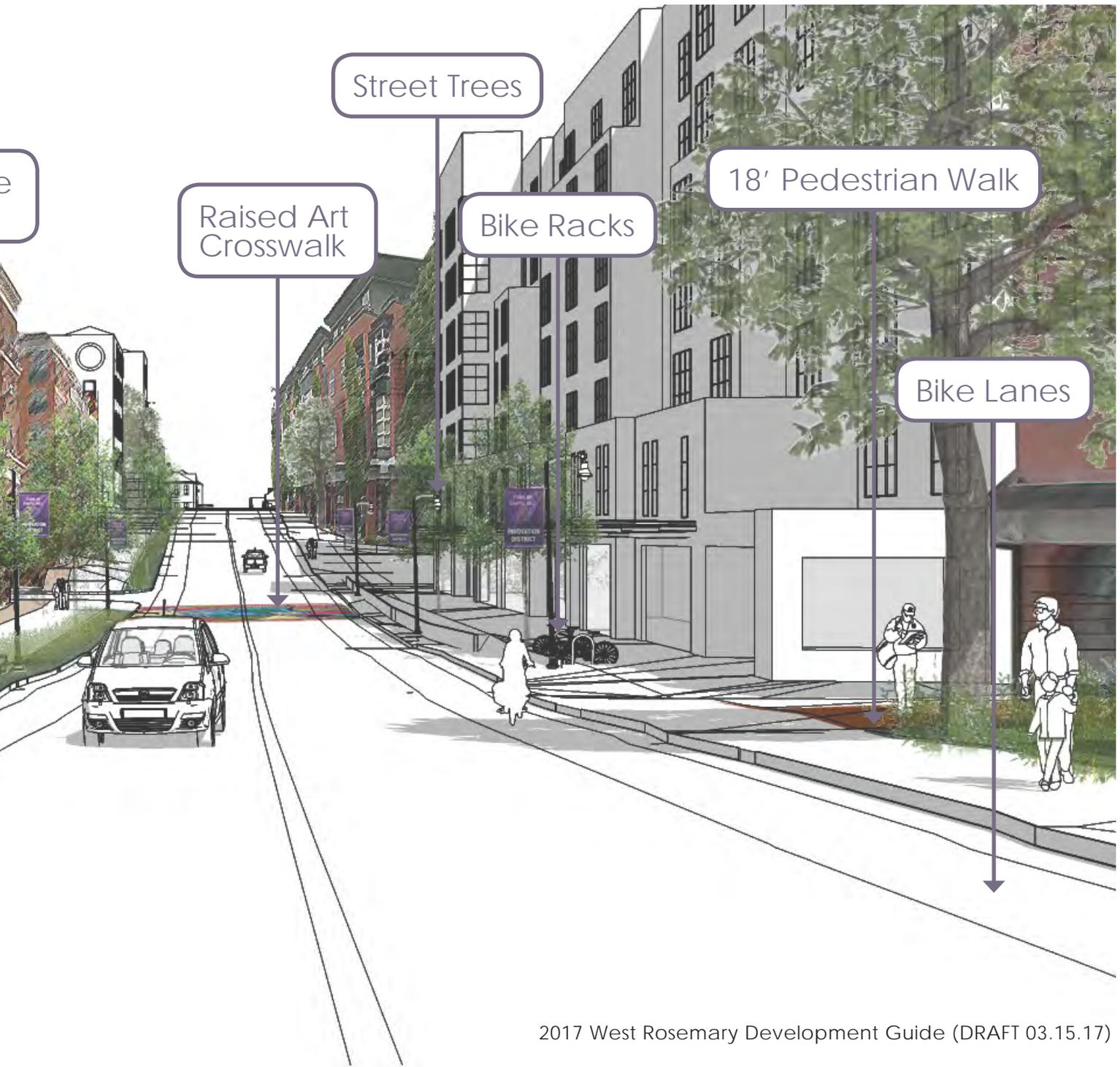
Landscape Buffer

10' Sidewalk





The image below shows how the street may look with future development. New development with ground floor retail or office would help to activate the street far better than surface parking does today. Adjacent pocket parks help blur open space and pedestrian zones together for additional community uses.



# Building Design



The Building Design section focuses on general building design standards that could be applied to new development. These standards are to help a building feel more humane in scale

Items covered within this section include:

- Defining the ground floor of a building
- Encouraging pedestrian activity along the ground floor
- Articulating the building facade
- Hiding unsightly infrastructure
- Transitioning to neighborhoods
- Application of the standards

# Building Design

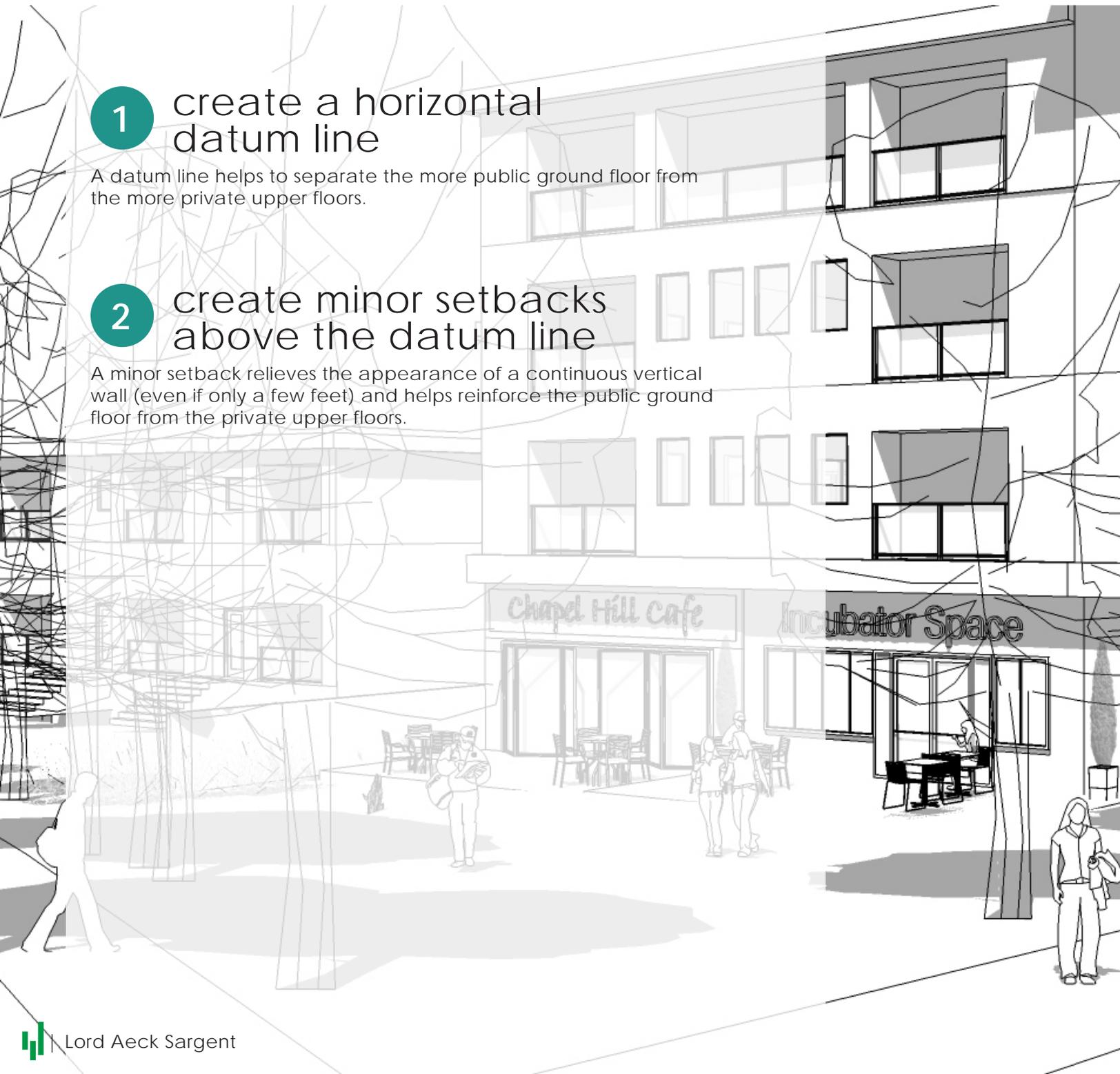
## Define the ground floor of buildings

### 1 create a horizontal datum line

A datum line helps to separate the more public ground floor from the more private upper floors.

### 2 create minor setbacks above the datum line

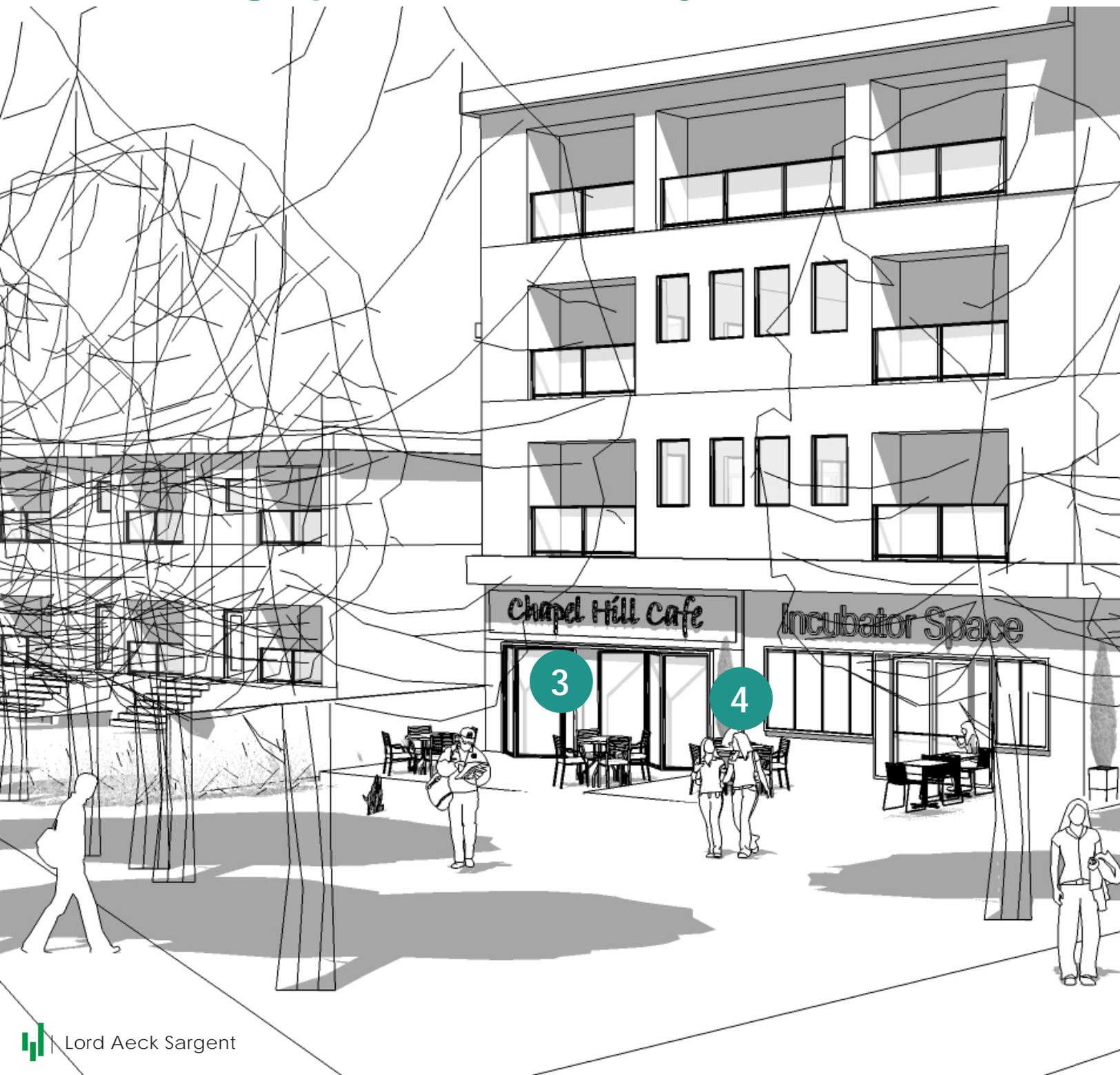
A minor setback relieves the appearance of a continuous vertical wall (even if only a few feet) and helps reinforce the public ground floor from the private upper floors.





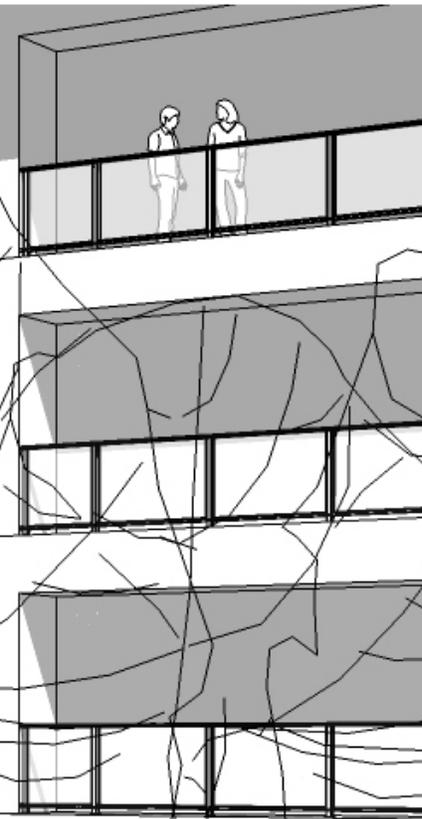
# Building Design

Encourage pedestrian activity





Buildings with open and inviting ground floors encourage a more active pedestrian realm. By requiring doors and windows facing the street developers provide something for pedestrians to look at and engage. This makes for a richer sidewalk and public realm experience, and also creates a more interesting interior space.



3

### require entries off streets or public spaces

90% of new buildings MUST have a functional entry onto the circulation network or other public space, such as a park or plaza (via sidewalk), but not a parking lot. – LEED ND, 2014.

4

### specify spacing between entries

Functional entries to the building MUST occur at an average of 75 feet or less along nonresidential or mixed use buildings or blocks. – LEED ND, 2014.

5

### specify minimum number of doors and windows

All ground-level retail, service, and trade uses that face a public space have clear glass on at least 60% of their facades between 3 feet and 8 feet above grade. – LEED ND, 2014.

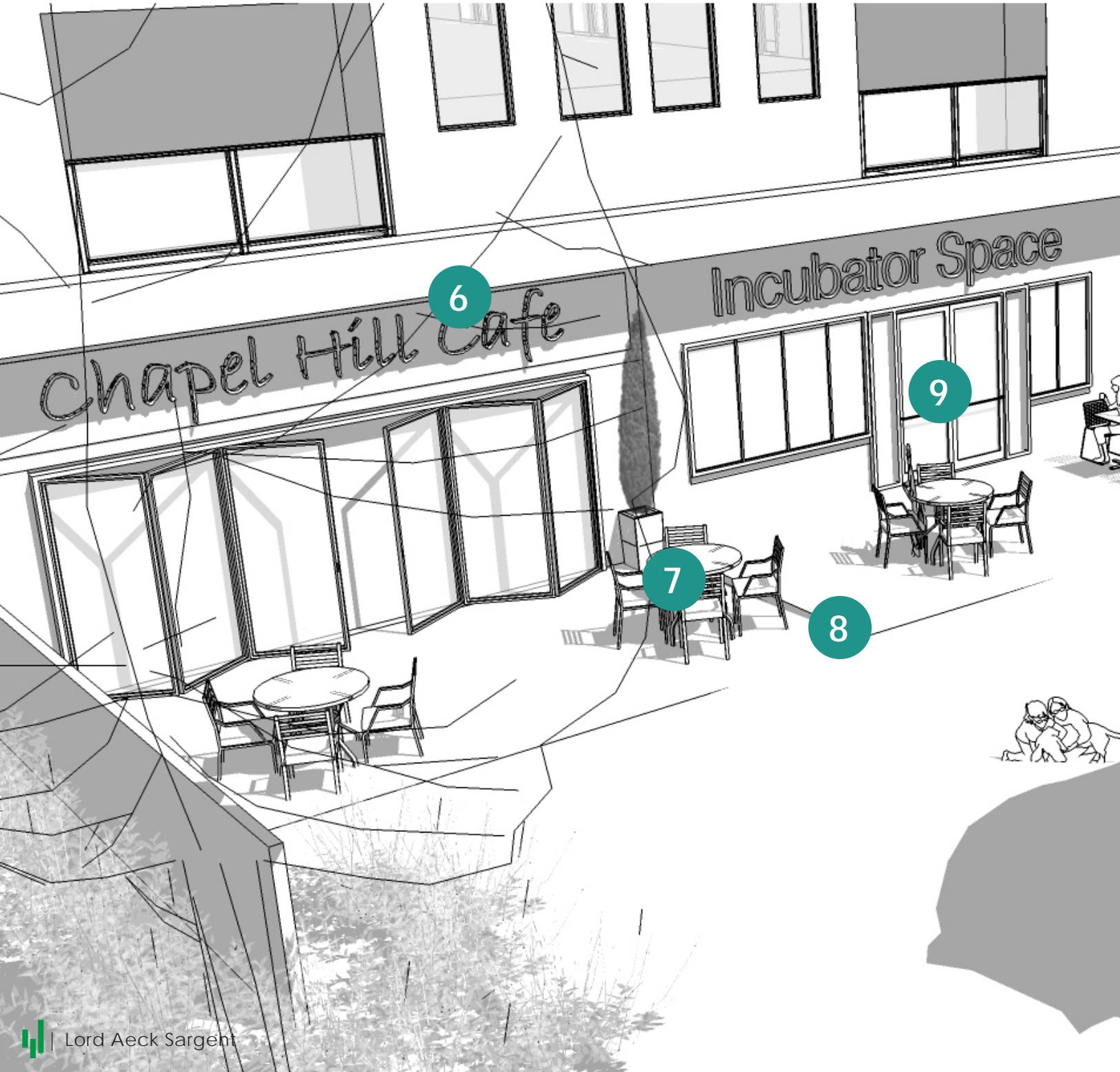


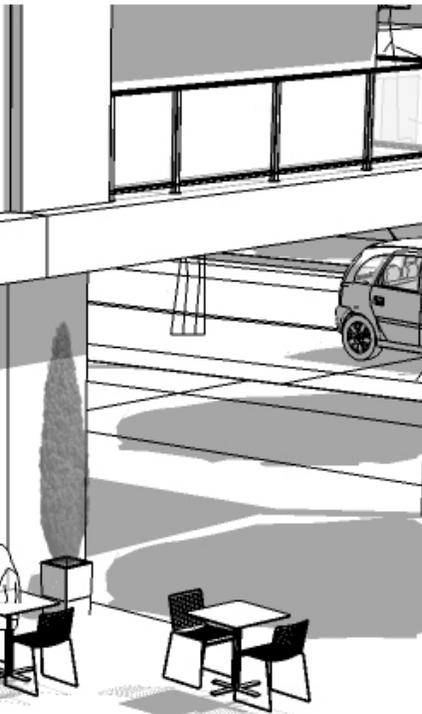
5

\* *LEED Reference Guide For Neighborhood Development*. 4th ed. Washington, D.C.: N.p., 2014. Print.

# Building Design

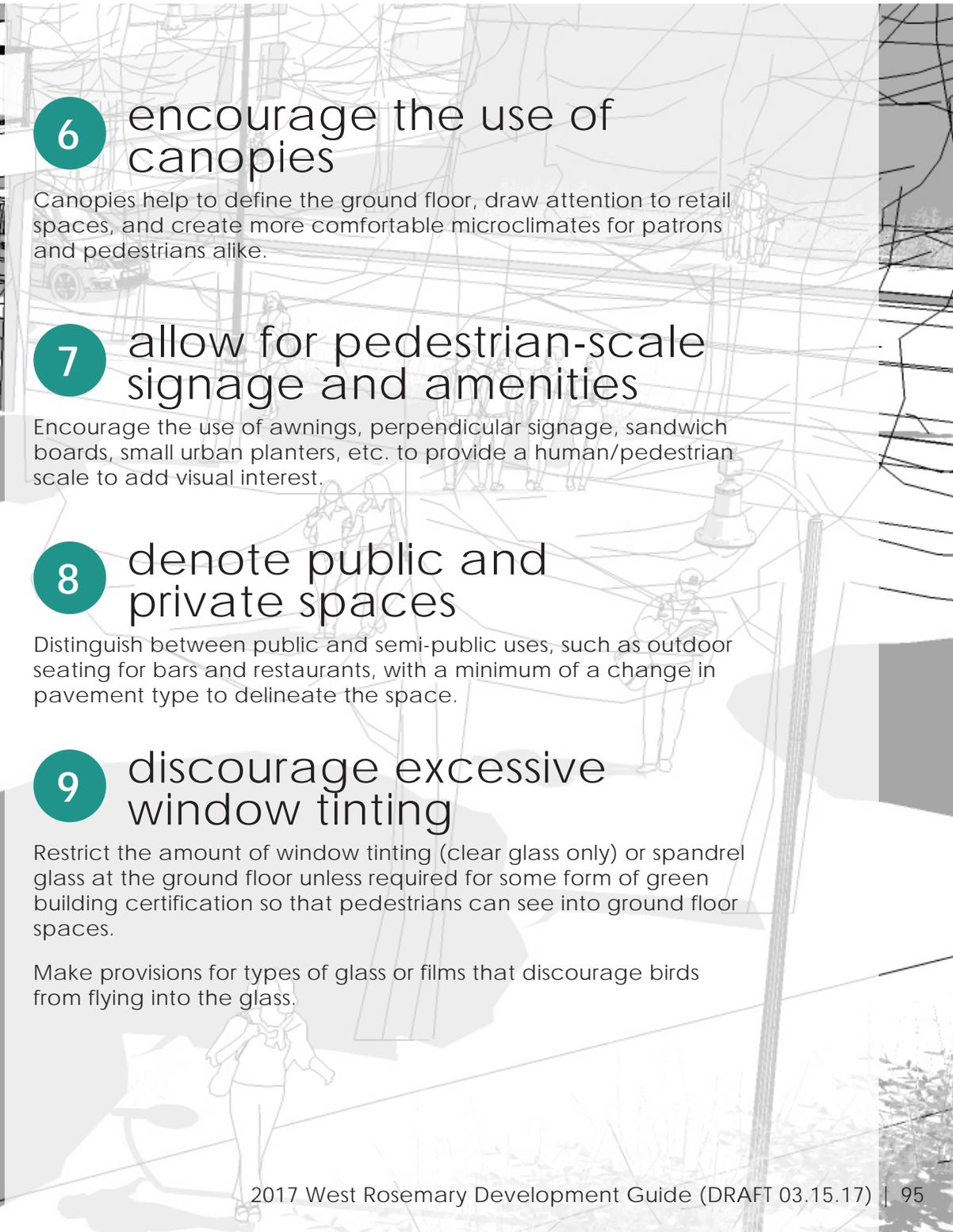
Encourage pedestrian activity, cont.





## 6 encourage the use of canopies

Canopies help to define the ground floor, draw attention to retail spaces, and create more comfortable microclimates for patrons and pedestrians alike.



## 7 allow for pedestrian-scale signage and amenities

Encourage the use of awnings, perpendicular signage, sandwich boards, small urban planters, etc. to provide a human/pedestrian scale to add visual interest.

## 8 denote public and private spaces

Distinguish between public and semi-public uses, such as outdoor seating for bars and restaurants, with a minimum of a change in pavement type to delineate the space.

## 9 discourage excessive window tinting

Restrict the amount of window tinting (clear glass only) or spandrel glass at the ground floor unless required for some form of green building certification so that pedestrians can see into ground floor spaces.

Make provisions for types of glass or films that discourage birds from flying into the glass.

# Building Design

## Articulate buildings

### 10 design for vertical bays

Design for vertical bays to help break up long stretches of massing and to provide the appearance of multiple buildings.

### 11 vary the upper cornice heights

Vary the upper cornice (roofline) line heights to avoid monotony and to provide the appearance of multiple buildings.

### 12 provide visual breaks in the facade / no blank walls

If a facade extends along a sidewalk, no more than 40% of its length or 50 feet, whichever is less, is blank (without doors or windows). – LEED ND, 2014.

### 13 accentuate visible corners

Celebrate and accentuate key, highly visible corners with towers/pedestrian entries, etc.



11

10

12

13

*\* LEED Reference Guide For Neighborhood Development, 4th ed. Washington, D.C.: N.p., 2014. Print.*

# Building Design

## Hide unsightly infrastructure

### 14 restrict parking to the rear/ interior/below buildings

Locate parking in the rear, internally or underneath development to hide view from public rights-of-way. Also, minimize the width and number of parking deck entries.

### 15 screen service or locate to the rear/underneath

Locate building service in the rear, internal to the site, underneath or within parking decks. E.g., no loading docks directly on West Rosemary. Provide adequate screening for service areas.

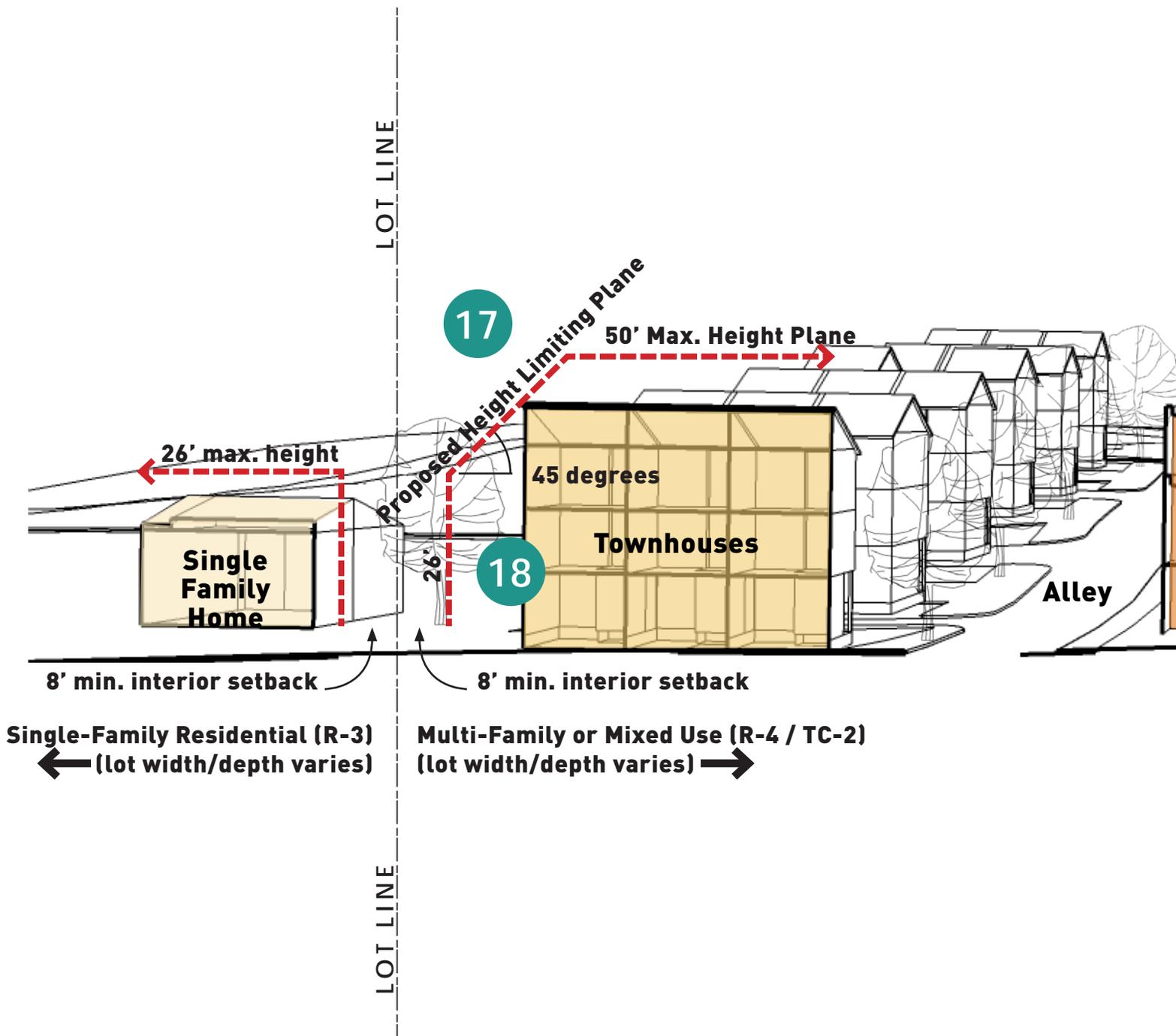
### 16 screen rooftop units

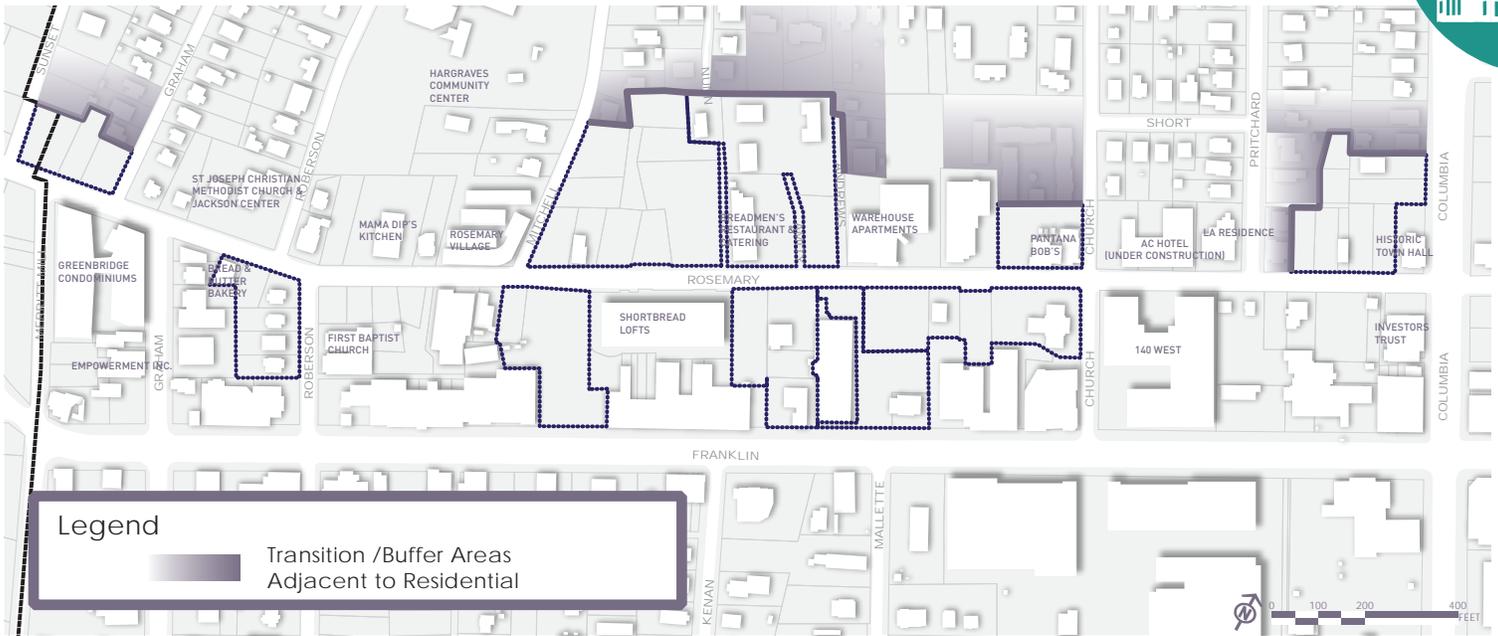
Hide or creatively screen unsightly rooftop units from the ground level..



# Building Design

Transition to neighborhoods





## 17 create transitional height planes

Create transitions to adjacent residential areas - Include step backs and transitions where butting up against residential zones. Building heights step down towards the neighborhood to reflect the smaller scale of the Northside Neighborhood.

In this example:

- You can't build within 8' of residential property line based on R-3 and R-4 interior setback regulations (there are no interior setback regulations for TC-2 or TC-3). This 8' is then used as a standard setback from the property line.
- Based on the maximum allowable height in the adjacent residential zone (in this case 20' in the R-3 with the NCD overlay) that becomes the maximum allowable height at the interior setback line.
- From there a 45 degree angle is set above which no structure is allowed to be built.
- Finally the overall maximum height plane is set by the zoning (in this case 40' in the TC-2 with the NCD overlay).

- ToCH Zoning Code

## 18 buffer adjacent residential

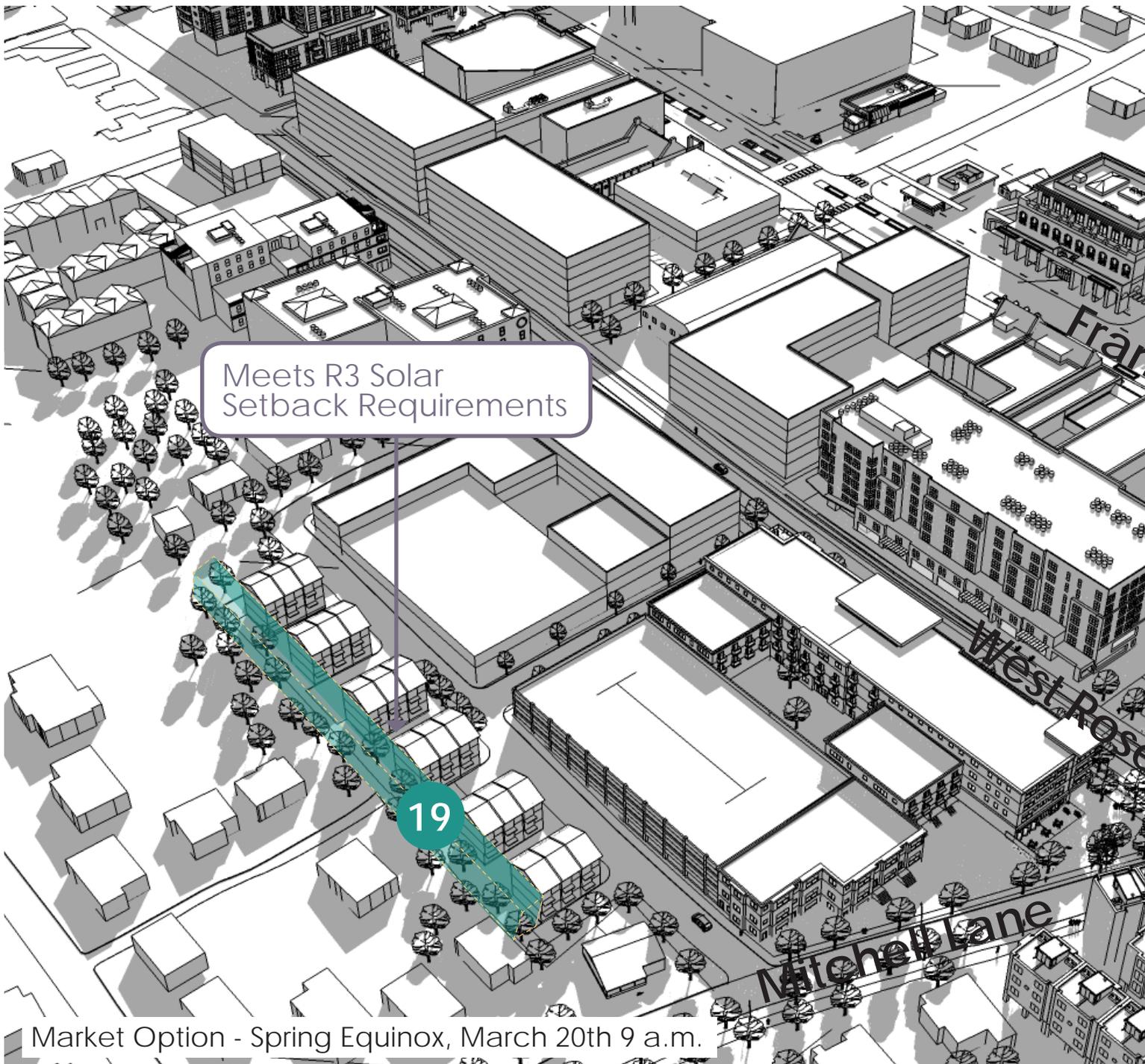
Buffers shall be required to separate a proposed development from adjacent major streets and different adjacent land uses or zoning designations in order to minimize potential nuisances such as the transmission of noise, dust, odor, litter, and glare of lights; to reduce the visual impact of unsightly aspects of adjacent development; to provide for the separation of spaces; and to establish a sense of privacy. - ToCH Zoning Code

In lieu of compliance with the above bufferyard and screening requirements, a developer may submit to the community design commission for its approval a detailed plan with specifications for landscaping and screening which will afford a degree of buffering and screening equivalent to or exceeding that provided by the above requirements.

### Multi-Family Apartments

# Building Design

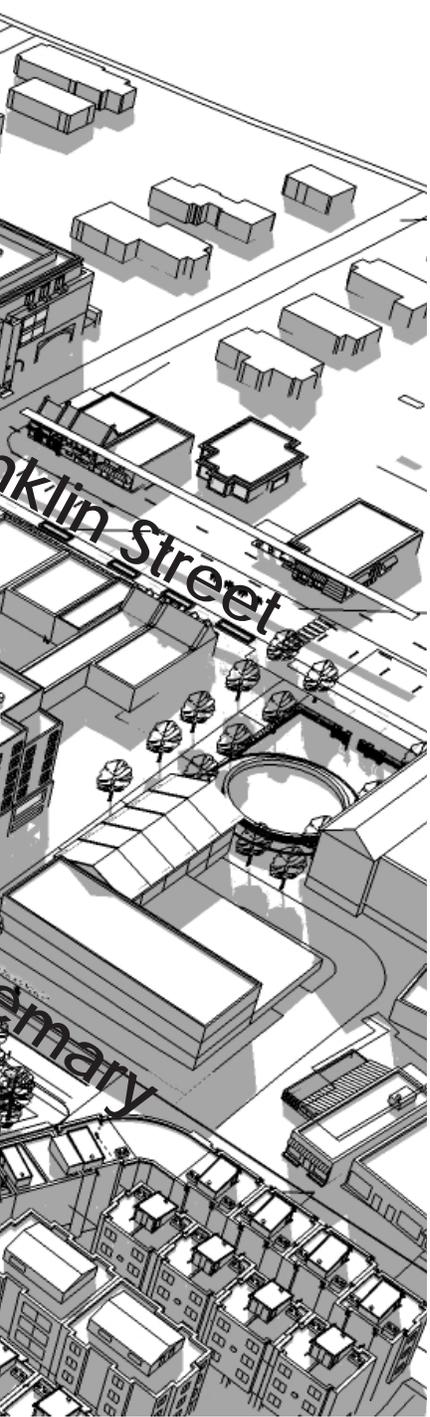
Transition to neighborhoods, cont.



Meets R3 Solar Setback Requirements

19

Market Option - Spring Equinox, March 20th 9 a.m.



## 19 follow solar setback regulations

Follow the solar setback requirements outlined in the Town's Zoning Codes.

Minimum Solar Setback - establishes a minimum setback measured from north lot lines. Where a solar setback and either a street or interior setback both apply to the same portion of a lot line, the required minimum setback shall be the greater of the two.

### Zoning Regulations

Based on the standard Chapel Hill Zoning Ordinance

	R-3 Residential	R-4 Residential	O-I Office-Institutional	TC-2 Town Center 2
Solar Setback (min. feet)	11'	9'	11'	0'

# Building Design

Case Study: Mitchell Lane Corner



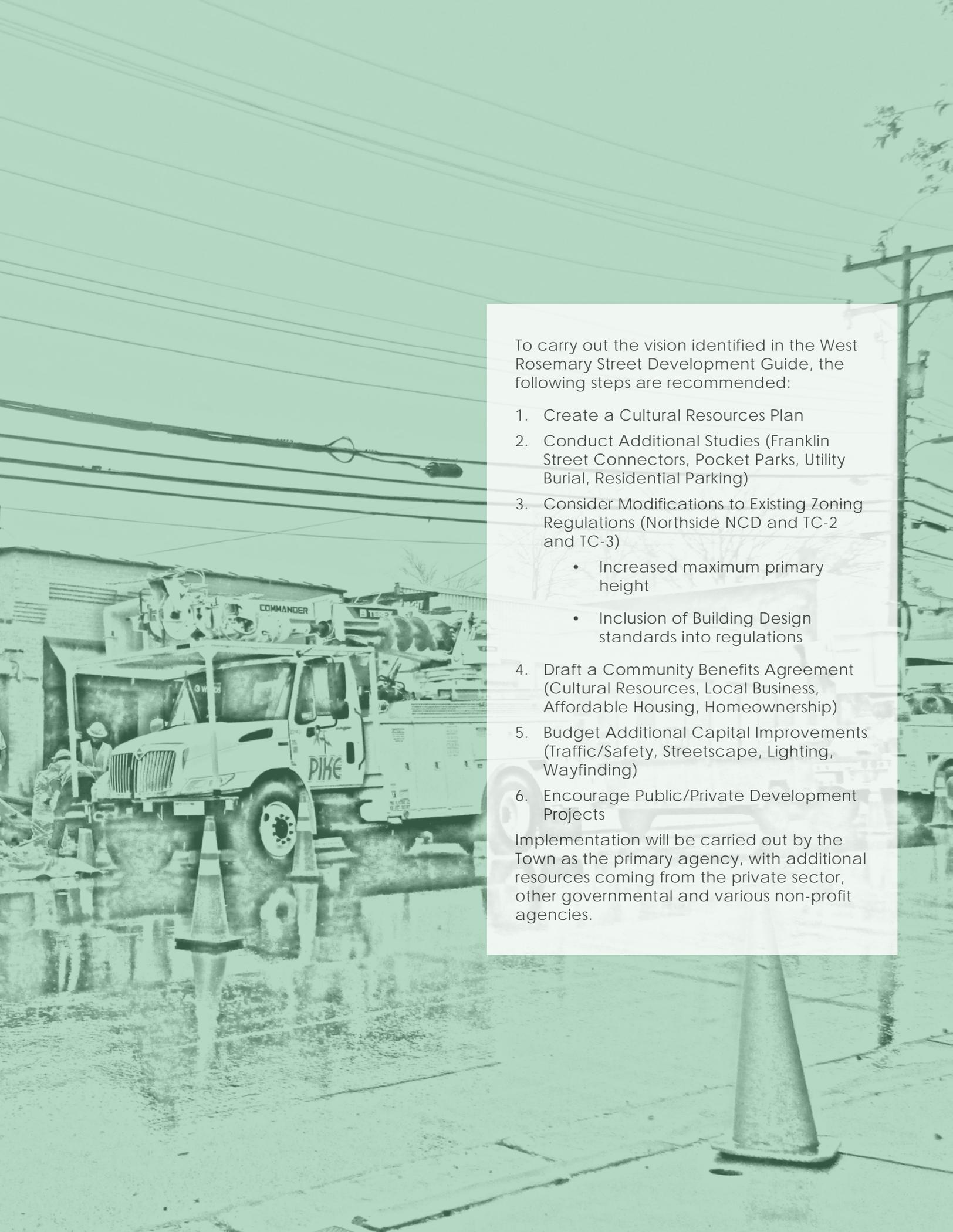


The image below represents the case study site at the corner of Mitchell Lane and West Rosemary Street that the team used to demonstrate how the development framework, public realm, and building design standards could be applied to a potential development site.



# Implementation Plan





To carry out the vision identified in the West Rosemary Street Development Guide, the following steps are recommended:

1. Create a Cultural Resources Plan
2. Conduct Additional Studies (Franklin Street Connectors, Pocket Parks, Utility Burial, Residential Parking)
3. Consider Modifications to Existing Zoning Regulations (Northside NCD and TC-2 and TC-3)
  - Increased maximum primary height
  - Inclusion of Building Design standards into regulations
4. Draft a Community Benefits Agreement (Cultural Resources, Local Business, Affordable Housing, Homeownership)
5. Budget Additional Capital Improvements (Traffic/Safety, Streetscape, Lighting, Wayfinding)
6. Encourage Public/Private Development Projects

Implementation will be carried out by the Town as the primary agency, with additional resources coming from the private sector, other governmental and various non-profit agencies.

# Implementation Plan

## Community Benefits

Project	Description	Implementation Step for the Town
<b>Local Business</b>		
Provide commercial tax incentives	Consider grant programs that are aimed at small business, such as those that assist with tenant improvements.	Look into additional grant programs
Create shared retail/office space	Providing incentives for the developer on the real estate side so that the developer is then able to create a larger “market hall” space or shared office space.	Further study incentive opportunities
Regulate for small-scale spaces	Encourage small scale retail spaces that can offer opportunities for smaller, local businesses. Typically 1,000 square feet or less.	Further study incentive opportunities
<b>Affordable Housing</b>		
Set affordable housing minimums for developers	Continue to enforce affordable housing minimums that are already covered by the existing Inclusionary Zoning ordinances.	
Provide density bonuses	<p>The Town has existing inclusionary zoning regulations which enable density bonuses to aid developers in supporting affordable housing requirements. The key will be in setting height limitations that give market value to additional height. This would need to be reviewed on a case by case basis.</p> <p>A density bonus in the West Rosemary corridor could have real value if base height is limited to four stories or less on the north side of the street.</p>	Further study inclusionary zoning with density bonuses as it applies to West Rosemary
Initiate a land write down	The Town could buy land and sell it below cost under the stipulation that the developer provide some type of community benefit - in this case designated locally-owned commercial space.	Look into opportunities for land write downs
Public financing of infrastructure	Possibly public financing of structured parking decks and/or public investment in streetscape, etc. that could serve to lower the overall development cost to the developer in exchange for community benefits.	Look into financing tools for structured parking
Encourage public/private development	Proactive assembly of a key development opportunity in partnership with a private or non-profit developer who would apply for tax credits and build an affordable housing development.	Work with a private developer

Project	Description	Implementation Step for the Town
Establish tax incentive policies	Taxes on a multi-family apartment complex deal are running around \$2k/unit for the area, at an average of 10 years that equates to around a \$30-40k/unit. Exploring ways under North Carolina law to minimize these taxes could be a powerful incentive.	Further explore tax incentives
Explore new resources for creating affordable housing	Look into the latest models to create affordable housing.	Further explore new affordable housing resources
Consider establishing a fund for large development projects	Create a fund to help developments of a certain size achieve larger affordable housing percentages.	Establish a large development fund
Evaluate existing policies that could be barriers to affordable housing	Consider refinements to policies that hinder the creation of affordable housing.	Evaluate existing policies
Evaluate incentives to encourage private sector partnerships	Create incentives to help successfully establish private partnerships to create new affordable housing.	Evaluate partnership opportunity incentives
<b>Homeownership</b>		
Encourage owner-occupancy through tax incentives	Maintain stability and affordability through grants to assist owner occupants with raising property taxes.	Create grants to assist with rising property taxes
Continue to use the community land trust model	A Community Land Trust (CLT) purchases property and holds it in perpetuity to be affordably sold to low-income families. If and when the time comes for the homeowner to sell, a buy-back clause allows for the CLT to repurchase the land at a reasonable price.	
Create down-payment assistance programs	Creation of a formalized program to assist potential qualified homeowners who may have the ability to make a monthly mortgage payment but not the means to have a substantial down-payment for the purchase of a home.	Assist in the creation of down-payment assistance program

# Implementation Plan



## Community Benefits, cont.

Project	Description	Implementation Step for the Town
<b>Homeownership, cont.</b>		
Sponsor the creation of new homes	Sponsor new homes through Habitat for Humanity and the “Neighbors of Northside” initiative - using students, downtown residents, and business owners to help in the building process.	Support the creation of new homes by non-profits
Utilize Town-owned land to develop new homes	Consider the inventory of Town-owned property for potential new home development.	Further study opportunities to use town-owned land for home development
<b>Cultural identity</b>		
Develop a community benefits agreement	A written agreement could be developed that spells out how community benefits of local business, affordable housing, and/or priorities from the Cultural Resources Plan could be a requirement of new development approvals.	Develop a community benefits agreement
Perform a cultural resources plan	This could include identification of historic and cultural physical features, ideas for new public space and public art, new cultural programming, communication measures, and community cultural priorities.	Develop a Community Benefits Agreement to be used with private developers
Encourage community-based public art	Integrate community-based public art within new developments and streetscapes.	Include in the capital improvements plan
	Continue the recommendations outlined in the 2016 Cultural Arts Plan.	2016 Cultural Arts Plan
Tie wayfinding with cultural identity	Use proposed wayfinding signage to also help highlight and celebrate the community character and identity. Tie this to both a future Cultural Resources Plan and the 2016 Cultural Arts Plan.	Include in the capital improvements plan



# Implementation Plan

## Development Framework, cont.

Project	Description	Implementation Step for the Town
<b>Preserve the vision of the Neighborhood Conservation District</b>		
Limit the height of buildings adjacent to neighborhoods	Consider a revision of the NCD and TC-2 to allow a maximum allowable building height of four floors, or 48 feet (assumed 15 feet for the ground floor and 11 feet for the remaining floors) on the north side of West Rosemary. The current standards only allow for 40' as the primary building height, which doesn't actually allow for a typical four stories in new construction.	Further evaluate regulatory review changes internally
<b>Traffic and Safety Improvements</b>		
Enhanced pedestrian zone	See <i>Public Realm Section</i> of the Implementation Plan	
Pedestrian connections from West Rosemary to Franklin	<b>1</b> Improve and expand the existing pedestrian connection between UNC's ITS Building and Shortbread Lofts (e.g. larger pathway, more generous lighting and plantings, seating areas, etc.).	Undertake a further study with property owners
	<b>2</b> Work with private property owner to create a more inviting pedestrian connection, that works with the adjacency to the Launch Building to enhance the space (e.g. added seating, lighting, plantings, art, etc.).	Undertake a further study with property owners



Project	Description	Implementation Step for the Town
<b>Traffic and Safety Improvements, cont.</b>		
<p>Vehicular connections from West Rosemary to Franklin</p>	<p><b>3</b> Consider one of two potential vehicular connections from Franklin Street to West Rosemary Street. It would be considered adventurous on many fronts to break up the megablock. Additional study of impacts to traffic and property will be required.</p>	<p>Undertake a further study with property owners</p>
<p>Manage access (driveways and curb cuts) along West Rosemary*</p>	<p><b>4</b> Limit the following to a single curb cut for any new development:</p> <ul style="list-style-type: none"> <li>• 600 north block (between Graham and Sunset)</li> <li>• 500 south block (between Roberson and Graham)</li> <li>• 300 north block (between Church and Mitchell) - could also be a small side street or alley. If possible align this curb cut with one across the street. These access points could eventually warrant a potential traffic signal.</li> <li>• Between 323-327 W. Rosemary Street's parking lots. If possible align this curb cut with one across the street. These access points could eventually warrant a potential traffic signal.</li> <li>• 311-317 W. Rosemary Street</li> <li>• Rear parking of 206-208 W. Franklin Street</li> <li>• 300 W. Rosemary Street</li> <li>• 100 north block (between Columbia and Pritchard) - no more than two curb cuts</li> </ul>	<p>Consider regulating inclusion by potential developer</p>

\*NOTE: Issues of egress and fire safety must be taken into account when considering improvements.



# Implementation Plan

## Development Framework, cont.

Project	Description	Implementation Step for the Town
<b>Traffic and Safety Improvements, cont.</b>		
Manage access (driveways and curb cuts) along West Rosemary*	<p><b>5</b> Limit the following to a single curb cut by creating a single shared drive:</p> <ul style="list-style-type: none"> <li>• 400 north block (between Roberson and Mitchell) parking between the parking lots at 490 and Mama Dip's</li> <li>• Between the First Baptist Church parking and 407 W. Rosemary</li> <li>• Between 347 W. Rosemary Street and UNC's N1 parking lot</li> <li>• 311-317 W. Rosemary Street</li> <li>• Rear parking of 206-208 W. Franklin Street</li> <li>• 300 W. Rosemary Street</li> <li>• 121-131 W. Rosemary Street</li> <li>• 109-121 W. Rosemary Street</li> </ul>	Develop an agreement with private property owners
Manage access (driveways and curb cuts) along West Rosemary*	<p><b>6</b> More clearly mark/signalize 140 West parking garage entry/exit (i.e., flashing signals for exiting vehicles, clear pedestrian striping and signalage, etc.).</p>	Develop an agreement with private property owners



Project	Description	Implementation Step for the Town
Control traffic	<p><b>7</b> Add a raised crosswalk at the following:</p> <ul style="list-style-type: none"> <li>• Across Graham Street</li> <li>• From the northwest corner of Graham Street and additional pedestrian crossing signage at the intersection</li> <li>• Across Pritchard Street</li> <li>• At intersections with the proposed pedestrian connectors from Franklin Street (standard crosswalks or art crosswalks)</li> </ul>	Add to capital improvements budget or consider for additional study
	<p><b>8</b> Consider study of pedestrian crossing enhancements, such as an additional raised crosswalk across Roberson on the south side or a HAWK beacon or other pedestrian crossing device given the potential conflicts associated with an offset intersection.</p>	
	<p><b>9</b> Study signal timing at the Church Street intersection. Signal priority to vehicles is encouraging pedestrians to jaywalk.</p>	
	<p><b>10</b> Initiate a traffic/transportation study at the corner of Columbia Street (e.g., signal timing, the use of turn lanes, pedestrian prioritization, etc.). Congestion is a concern at all levels.</p>	
	<p><b>11</b> Consider study of pedestrian crossing enhancements, such as a HAWK beacon where the pedestrian connector intersects with Franklin Street.</p>	

\*NOTE: Issues of egress and fire safety must be taken into account when considering improvements.



# Implementation Plan

## Development Framework, cont.

Project	Description	Implementation Step for the Town
<b>Community Space / Landscape Enhancements</b>		
Continue Downtown Streetscape improvement projects	Continue with the improvements outlined in the 2009 Chapel Hill Streetscape and Lighting Master Plan	
Encourage plantings on private property to enhance streetscape	<b>1</b> Pursue an incentives grant through the Town, Tree City USA Community Improvements Program, or a similar agency, to enhance the street tree plantings on private property where space is not available within the existing streetscape.	Pursue grant funding and work with private property owners
Require sufficient street plantings in all new developments	<b>2</b> Require street plantings per the specifications outlined in the <i>Public Realm</i> section for all new development.	Consider regulating inclusion by potential developer
Improve pedestrian connections to Franklin	<b>3</b> Improve the existing pedestrian connection between UNC's ITS Building and Shortbread Lofts (e.g. canopy trees, planters, etc.), and connect to the expansion of the ITS pocket park.	Undertake a further study with property owners



FRANKLIN

Project	Description	Implementation Step for the Town
Improve pedestrian connections to Franklin	<b>4</b> Work with private property owner to create a more inviting pedestrian connection, that works with the adjacency to the Launch Building to enhance the space (e.g. added plantings, etc.).	Undertake a further study with property owners
Creation of pocket parks or enhanced open space	<b>5</b> Encourage the creation of public open space at the corner of Mitchell in the event that corner is redeveloped.	Work with private developer
	<b>6</b> Include enhanced open space to frame the Historic Town Hall and provide a buffer in the event of future development.	Include in capital improvements plan
Enhance existing open space	<b>7</b> Redesign the open space adjacent to UNC's ITS Building, including potential expansion to the parking lot directly to the northeast, and make open to the public.	Undertake a further study with property owners



# Implementation Plan



## Development Framework, cont.

Project	Description	Implementation Step for the Town
<b>Green / Stormwater Overlay</b>		
Include rain gardens with street improvements	Work with the private developer and the Town to pursue grant funding for the design and inclusion of rain gardens along any new streetscape improvements and within private development, pursuant to requirements outlined for the allowable levels of stormwater runoff from a site.	Include in capital improvements plan
Include stormwater features within pocket parks	Encourage stormwater features as part of any pocket park or public open space design.	Work with private developer
Encourage green roofs	Encourage the use of green roofs on new developments through incentive programs.	Work with private developer
Permit rainwater collection in cisterns	Encourage developers to utilize storage collection systems to harvest stormwater and reuse it for irrigation or building system functions (e.g. toilets, cooling systems, etc.).	Work with private developer
<b>Parking resolutions</b>		
Reevaluate residential permit parking	Revisit the Town's existing residential permit parking zones and regulations based on current and projected development	Enter into additional study
Create structured parking	In exchange for the developer constructing a parking deck with an excess of spaces the municipality agrees to lease a certain number of spaces back for public parking.	Work with private developer
	Town bonding to develop structured parking with long term lease with developer to fulfill the developer's lender's parking requirements for dedicated parking.	Work with private developer
Create clearer wayfinding to parking	Future parking could better be served through clearer wayfinding throughout Downtown which would guide people to public parking locations.	Include in capital improvements plan
<b>Transit Enhancements</b>		
Enhance transit stops	Provide shelters and real-time schedule information, or at a minimum benches, at all new transit stops, and at any existing transit stops where space allows.	Include in capital improvements plan



# Implementation Plan



## Public Realm

Project	Description	Implementation Step for the Town
Continue successful Downtown Streetscape improvement projects	Continue with the improvements outlined in the 2009 Chapel Hill Streetscape and Lighting Master Plan	
<b>Enhance the pedestrian experience</b>		
Require street plantings and buffers	Require that all new development provide street trees along at least 60% of the total planned block length, between the vehicle travel way and walkway.	Work with Private Developer
	Require that all new development provide trees of similar but not identical form, planted a maximum of 30 feet apart (excluding driveways and intersections). Each tree should have no branches lower than 7 feet in height at the time of planting. Further "limbing up" to 10 to 16 feet as part of a regular pruning program will encourage the formation of a high continuous canopy.	Work with Private Developer
Provide sufficient space for pedestrians	Require that all new development provide clear pedestrian zones, a minimum of 10 feet wide along major retail or mixed-use blocks (such as Rosemary Street) and at least 5 feet wide on all other blocks.	Work with Private Developer
Provide continuous sidewalks	Require that all new development provide continuous sidewalks along 90% of any new circulation block length. Alleys may be exempted.	Work with Private Developer
<b>Make public space more inviting</b>		
Provide sufficient amenities	Encourages placement of benches at any new bus stops, in pocket parks, and in conjunction with any existing or new amenable businesses where room allows. Discourage the placement of benches in areas where people are unlikely to want to gather or sit.	Work with private developer
Add welcoming and safe lighting	Require that any additional lighting comply with the Illuminating Engineering Society of North America (IESNA) standards	Work with private developer

Project	Description	Implementation Step for the Town
<b><i>Ease pedestrian circulation</i></b>		
Clearly mark paths and wayfinding	Increase the number of path markings and wayfinding/informational signage to points of interest and key pedestrian connections. This signage and wayfinding can also be tied to the cultural identity of the neighborhood.	Include in capital improvements plan
Provide enhanced pedestrian crossings	Consider raised and enhanced crosswalks and intersections at the additional proposed crossing enhancements outlined previously (in the Development Framework section).	Include in capital improvements plan
<b><i>Hide unsightly but necessary features</i></b>		
Screen infrastructure	Require that all new development mitigate the visual impact of necessary infrastructure through architectural screening techniques or by placing infrastructure behind a seal-able door.	Work with Private Developer
Hide parking	Require that all new development lessen the visual impact of parking by placing it in underground or screened structured parking, or by placing it off the main roadway, behind the building and wrapping it.	Work with Private Developer
Bury utility lines	<p>Work with Duke Energy to bury the overhead utility lines that are impeding successful street tree canopies and detracting from the visual aesthetics of the street.</p> <p>This would require a system-wide investment by the Town and although it is cost-prohibitive for a single project the impact would be immense (\$9-13M at the last estimate). Residents and business owners echoed their support.</p>	Undertake further feasibility study
Reduce the number and size of curb cuts	Require that within all new development no more than 20% of the block length of the circulation network is faced directly by garage and service bay openings.	Undertake further feasibility study / Work with private property owners

# Implementation Plan



## Building Design

All of the following are assumed to on the private developers end with oversight and approval by the Town.

Project	Description
<b>Define the ground floor of buildings</b>	
Create a horizontal datum line	Require that all new development is constructed with a horizontal datum line that helps to separate the more public ground floor from the more private upper floors.
Create minor setback above the datum line	Require that all new development contains minor setbacks in the facade to relieve the appearance of a continuous vertical wall and helps reinforce the public ground floor from the private upper floors.
<b>Encourage pedestrian activity</b>	
Require entries off streets or public space	Require that 90% of new buildings within any new development must have a functional entry onto the circulation network or other public space, such as a park or plaza (via sidewalk), but not a parking lot.
Specify spacing between entries	Require that within all new development, functional building entries must occur at an average of 75 feet or less along nonresidential or mixed use buildings or blocks.
Specify minimum number of doors and windows	Require that within all new development all ground-level retail, service, and trade uses that face a public space have clear glass on at least 60% of their facades between 3 feet and 8 feet above grade.
Encourage the use of canopies	Encourage the use of canopies to help define the ground floor, draw attention to retail spaces, and create more comfortable microclimates for patrons and pedestrians alike.
Allow for pedestrian-scale signage and amenities	Encourage the use of awnings, perpendicular signage, sandwich boards, small urban planters, etc. to provide a human/pedestrian scale to add visual interest.
Denote public and private spaces	Distinguish between public and semi-public uses, such as outdoor seating for bars and restaurants, with a minimum of a change in pavement type to delineate the space.
Discourage excessive window tinting	Restrict the amount of window tinting (clear glass only) or spandrel glass at the ground floor unless required for some form of green building certification so that pedestrians can see into ground floor spaces.
<b>Articulate buildings</b>	
Design for vertical bays	Require that within all new development vertical bays are designed to help break up long stretches of massing and to provide the appearance of multiple buildings.



<b>Project</b>	<b>Description</b>
Vary the upper cornice heights	Require that within all new development the upper cornice line heights vary to avoid monotony and to provide the appearance of multiple buildings.
Provide visual breaks in the facade	Require that within all new development, if a facade extends along a sidewalk, no more than 40% of its length or 50 feet, whichever is less, is blank (without doors or windows).
Accentuate visible corners	Encourage the celebration of key, highly visible corners with towers/pedestrian entries, etc.
<b><i>Hide parking and service</i></b>	
Restrict parking to the rear/ underneath buildings	See Public Realm section
Screen service or locate to the rear/ underneath	See Public Realm section
Screen rooftop units.	Hide or creatively screen unsightly rooftop units from the ground level..
<b><i>Transition to neighborhoods</i></b>	
Create transitional height	Require that all new development transition to adjacent residential areas - Include step backs and transitions where butting up against residential zones. Building heights step down towards the neighborhood to reflect the smaller scale of the Northside Neighborhood.
Buffer adjacent residential	Require that all new development buffer residential based on buffer regulations outlined in the Town's Zoning Codes.
Follow solar setback regulations	Follow the solar setback requirements outlined in the Town's Zoning Codes